



**REDESIGN OF TRIPOLI INTERNATIONAL
AIRPORT PARKING**

THESIS

**Submitted as partial fulfilling of the Requirement for The Research Methodology
Lecturer of master of Civil Engineering
Diponegoro University**

**By
Massuod Ali Mohamed Dakeel
No Of Student : 21010111409016**

**Master Program of Civil Engineering
Post Graduate Program
Diponegoro University
Semarang
2013**

APPROVAL

**REDESIGN OF TRIPOLI INTERNATIONAL
AIRPORT PARKING**

Prepared by:

Massuod Ali Mohamed Dakeel

NIM: 21010111409016

Maintained in front of Examiners team on:

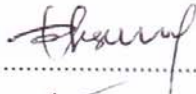
April 3, 2013

This thesis has been accepted as one of the requirements for obtaining
A Master Degree in Civil Engineering

Tim Examiners:

1. Dr .Bagus Hario Setiadji ST ,MT

(Supervisor)

: 

2 . Dr. Ir. Bambang Riyanto, DEA

(Co Supervisor)

: 

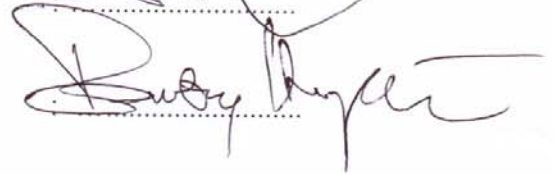
3 . Kami Hari Basuki ,ST, MT

(Member 1)

: 

4 . Dr. Ir. Bambang Haryadi , MSC

(Member 2)

: 

Semarang, April 2013
Diponegoro University
Post Graduate Program
Master of Civil Engineering

Head of Program

Dr. Ir. Bambang Riyanto, DEA
NIP. 19530326 198703 1 001

ABSTRACT

Study the issue is to evaluate, design and parking station travelers Tripoli International Airport, which is the largest airport in Libya at the moment and due importance to being the largest area of an airport in Libya, where Tripoli, capital of Libya, and the population density is equivalent to 2 million people. The objectives of this study are to evaluate the parking terminal and propose a solution. This chapter III discusses the methodology used in this research. The methodology in this research includes research preparation and literature review, data collection, analysis of the data and conclusion, as shown in the following flow chart, the description of each step is presented in subsequent sections. Parking passenger station to the airport in Tripoli existing need to develop and increase the capacity for the number of vehicles from the current capacity is increasing the capacity of the situation through the use of new additional space will be extended space to the north and re-planning position at an angle of 90 now under implementation side parts and which represent new passenger terminals.

Parking-based scheme angle of 90° The reason for the lack of sufficient capacity for the car park to reach the number of accumulation vehicles next position to the maximum value and that traverse the full capacity of the situation and thus can be attributed the rise in the number of cars as a result of passenger flow, which causes suffocation in the parking lot and not the ability to carrying excess number of cars that under the circumstances this time there are not enough positions for the operation of vehicles coming to him than we had re-design of this case . After previous conclusions and see the results of data analysis, where it was a peak hour start from (9 am to 10 am) which is cause of the congestion in Tripoli international airport parking and irregular parking system, found insufficient of parking capacity during this period which need to solve this problem so recommend to re-design Parking Tripoli International Airport to provide all facilities necessary needed of parking and must make special parking for public transport also places allocated for people with special needs and need to increase the capacity of terminal parking meet the needs of the delegates to the airport as well as taking into account the new terminals, which is under construction now.

Further increase the parking capacity, in anticipation of the increasing number of vehicles to be parked in the future. This change will increase the number of parking spaces for more vehicles than the current parking demand.

Data were collected vehicular traffic during the 12-hour part of the requirements analysis of these data required using formulas analysis contained in Chapter II of the analysis that we have gained as a result of the position is sufficient number of cars coming to him, which requires a preplanning of the car park at the corner of about 90° to provide a number more places the situation and also spaces periods less and thus can be attributed the rise in the number of cars as a result of passenger flow, which causes suffocation in the parking lot and not the ability to carry a number of vehicles in excess of that in the circumstances this time there are situations is not enough to run vehicles expatriate than we redesign this case.

TABLE OF CONTENT

Title	i
Approval	ii
Ratification	iii
Official Note	iv
Declaration Authenticity	v
Preface	vi
Table of Contents	vii
List of Table	xi
List of Figure	xii
Abstract	xiii
CHAPTER I INTRODUCTION	1
1.1 Background	1
1.2 Problem Statement	3
1.3 Objectives	5
1.4 Scope Of The Study	5
1.5 Organization Of Thesis	6
CHAPTER II LITERATURE REVIEW	7
2.1 Parking	7
2.2 Definitions Transportation System	7
2.3 Transportation and Land Use Relationships	8
2.4 Parking Studies	8
2.5 Parking Surveys	9
2.6 Parking Maximum	10
2.7 Method of Determining the Provision of Parking Spaces	10
2.7.1 The Method Focuses on the Number of Trips by Car .	10
2.7.2 The Method Focuses on the Capacity Road-Related Activity Center	11
2.7.3 The Method Focuses on the Number of Vehicle Ownership	11

2.7.4	Method of Volume Cordon Studies with a Maximum Accumulation	11
2.8	Effects Of Parking	12
2.9	Parking Requirements	13
2.10	On Street Parking	13
2.11	Off Street Parking	17
2.12	Parking Lot Design	17
2.12.1	General	17
CHAPTER III	RESEARCH METHODOLOGY	22
3.1	Introduction	22
3.2	Types Of Data Needed	23
3.3	Parking Data Collection Methods	24
3.4	Time To Collect Secondary	24
3.5	Parking System At The Tripoli International Airport Is To TheFollowing	24
3.6	Supply Data	24
CHAPTER IV	RESULTS AND ANALYSIS	26
4.1	Data Collection	26
4.2	Demand of parking	30
4.3	Use of Another Parking System	32
4.4	Creating Rules of Parking Time Allowed.....	33
CHAPTER V	CONCLUSIONS AND RECOMMENDATIONS	36
5.1	Conclusions	36
5.2	Recommendations	37
Appendix		
References		

LIST OF TABEL

Table. 4.1	Number of Vehicles Arrived Hourly	27
Table. 4.2	Number of Vehicles Departed Hourly	28
Table 4.3	the Number of Vehicles Parked based on Each Parking Time	28
Table 4.4.	The Accumulation of Vehicles Parked	31
Tabel A.1	Car survey	39

LIST OF FIGURE

Figure 1-1	Terminal Building	1
Figure 1-2	Tripoli International Airport Area	2
Figure 1-3	Taxi Stands Front Of Terminal	3
Figure 1-4	New Terminal Under Construction	4
Figure 1-5	Cars in front terminal building	4
Figure 1-6	The main Gate of Tripoli International Airport	5
Figure 2-1	Shows Parking Exemplary	7
Figure 2-2	Illustration Of Parallel Parking	14
Figure 2-3	Illustration Of 30° Parking	15
Figure 2:4:	Illustration of 45° parking.....	15
Figure 2:5:	Illustration of 60° parking	16
Figure 2:6:	Illustration of 90° parking	16
Figure 2-7:	Illustration of off-street parking	17
Figure 3.1	Methodology of The Study	22
Figure 4.1	land use of parking	26
Figure 4.2	Existing parking space	27
Figure 4.5.	Number Parked based on Each Parking Time of Vehicles	29
Figure 4.6	Accumulation of arrived & departure vehicle	31
Figure 4.7.	The Line Graph of the Accumulation of Vehicles Parked	32
Figure 4.8	Optimal Parking Layout for 60°-angular Parking System	34
Figure 4.9	New parking design	35

CHAPTER I

INTRODUCTION

1.1 Background

Tripoli International Airport is the largest airport in Libya saluting located in the city of Tripoli (the town of Bin Ghashir) within 34 kilometers (21 miles) to the south from the center of the city of Tripoli. Extends from the airport to the estimated value of the total area of 12148194 square meters, and this space include all of the current airport terminal is currently serving international and domestic flights. Absorptive capacity of up to 3 million passengers per year. And dealing with passenger airport 2100000 in 2007, and one million passengers more than 3 in 2008 Within the past few years to build two new terminals on the total area allocated to the airport and all the new plants, with an area of 162,000 square meters (1.74 million square feet) in size, which will become the overall capacity of the airport to 20 million people.

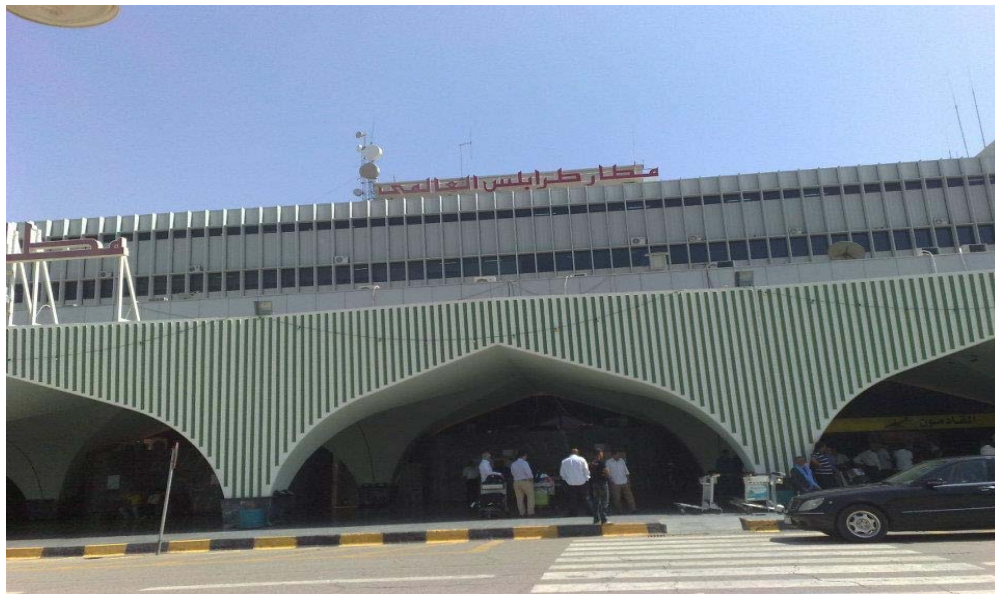


Figure 1-1 Terminal Building

Hall of the old terminal is a five-storey building area of 33,000 square meters (360.00 square feet), and the airport is operated within 24 hours a day. But there are plans to build an airport hotel for transit passenger service. The new project aims to build two

new terminals at the airport (East and West Terminal) on both sides of the existing international terminal, see Figure 1-2 illustrates this .

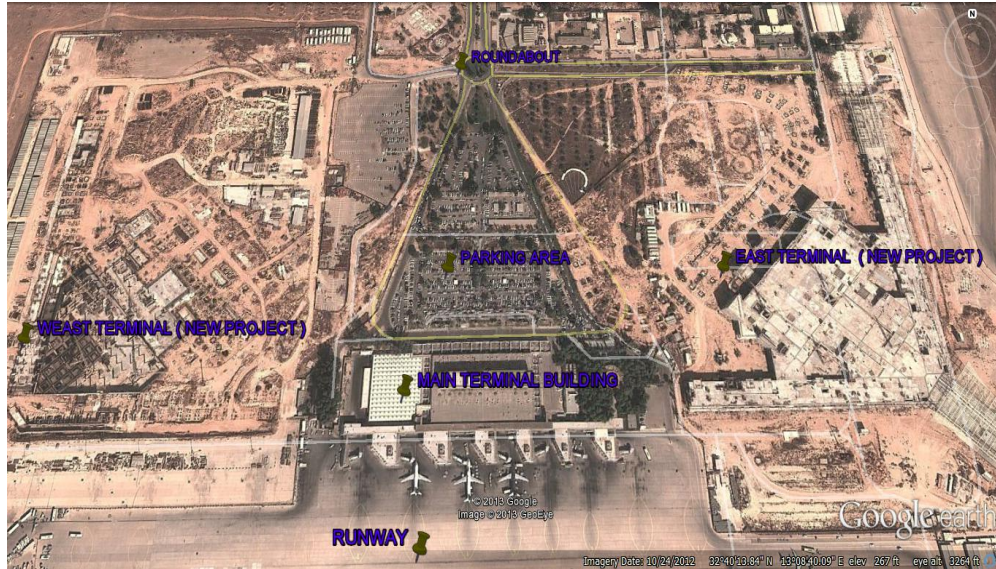


Figure 1-2 Tripoli International Airport Area

Where it was laying the foundation stone for the project in December 2007 and start construction work on the spot of a new east and west terminal in 2008 and until the beginning of 2011 has stopped working suddenly, it was not completed the construction because of Libya war and so far have not resumed work on new the project .We note that the schedule of a new project is 5 years for the implementation . Parking of old terminal current is the basis for the study of the proposed problem is capacity of terminal parking , it is not enough for parking a number of cars coming to its and we can put the details of this problem is due to lack of places allocated for people with special needs and also there is not a station for taxis and buses and there is some of reasons including this problem as is improper planning of the parking and Figure shows and some taxis parking in front of terminal building despite a traffic signal, which does not allow parking in front of the terminal building.



Figure 1- 3 Taxi Stands Front Of Terminal

Planning for parking at a 90-degree angle, accommodate 400 cars and circulation is complicated inside parking as overcrowding as cars cross main gate when it enter or exit of one gate .

1.2 Problem Statement

Tripoli International Airport is the main airport in the Libyan capital Tripoli, where is the southern gate of the continent European. As mentioned previous in other words, Libya is the heart of the African continent and that due to its geographical location where the longest coastline extending north Africa for this reason it must develop and redesign the airport style modern prevailing in the world so that it can increase the capacity of the airport to 20 million passengers per year which is currently still under construction.



Figure 1-4 New Terminal Under Construction

The basic problem in question is to re-develop and design the car park as well as the allocation of sufficient space to create a station public transport including buses and taxis. Where the existing car park has several problems which are summarized in the following:

1. insufficient parking as a result of bad planning of the area that not to follow the specifications and see figure 1-5 there are many cars in front terminal building as crowded and traffic signal not allowance to parking there .



Figure 1-5 Cars in front terminal building

2. non-allocation within the station yard private airport for bus and taxi.
3. Non-provision of transport of people with special needs.

4. There is no automatic control, which enables to conduct the regulatory process for the passage of cars smoothly and regularly.

5. Considered also from one existing problems is position of passenger terminal parking as the lack of organizational management of vehicular traffic and follow the behavior of motorists in the car park where the behavior of some people is one of the reasons this problem and is the lack of parked the vehicle in places allocated sense the vehicle at out of planning for the car park, which is also what it means not to follow people and obedience to follow-up the laws within the car park , which requires structural re-organization of vehicular traffic within the administration position and having to follow all laws and regulations.



Figure 1-6 The main Gate of Tripoli International Airport

1.3 Objectives

The objectives of this study are to evaluate the parking terminal and propose a solution.

1.4 Scope Of The Study

The proposed research topic is a redesign of parking in Tripoli International Airport, which can be described as follows: -

1. Tripoli International Airport is located South of the capital Tripoli, about 34 km away.
2. Parking terminal is located in the front of the airport building and is bounded by faced along the New Tripoli Airport project.

3. The method used to analyze data vehicular traffic in this study is descriptive statistics based on parking theory.

1.5 Organization Of Thesis

To facilitate easy and comprehensive, reading the results of this study will need to organize as follows:

A. CHAPTER I (Introduction)

It contains a description of the background research, the research aims and objectives, constraints and systematic problems of writing.

B. CHAPTER II (literature review)

It contains a description of the basics of theories related to the parking theories and its characteristics, statistical methods, and previous similar studies have ever done.

C. CHAPTER III (Research Methodology)

It contains a description of the mindset of research, stages and procedures of research and analytical methods used.

D. CHAPTER IV (Data Analysis and Discussion)

This section contains a description of the data results of research is accompanied by analysis and discussion of the nature and trends of the results of the study.

E. CHAPTER V (Conclusions and Recommendations)

This section contains the conclusions that can be drawn from the results of analysis . It is also presented suggestions for the application of the possibility of further study

CHAPTER II

LITERATURE REVIEW

2.1 Parking

Parking is one of the major problems that is created by the increasing road traffic. It is an impact of transport development. The availability of less space in urban areas has increased the demand for parking space especially in areas like Central business district. This affects the mode choice also. This has a great economical impact.



Figure 2 – 1 Shows Parking Exemplary

2.2 Definitions Transportation System .

Transportation system is an interaction that occurs between the three inter-related components of the system and affect, the activity, transportation networks, and current (flow). For example, the current transport from one region to another occurs due to activity (economic, social, political, etc.). Mean while, the onset of flow due to infrastructure and means of transport between both regions.

2.3 Transportation and Land Use Relationships .

Because land use is a representation of the type of human activity, it can be said that the inter-land use relationships that will always happen is a form of connection between human activities with each other. In an attempt to meet the need for their activities, humans must move from one land to another land . Shifting that is referred to as the trip (trip). If you travel a certain path using modes and then the trip is called traffic. Historically, land has always recognized a close relationship with transport. Changes in land use always leads to changes in the transportation system and vice versa. Connectedness between the two components are often referred to as the cycle of land use and transportation (land use-transportation cycle) .

2.4 Parking Studies .

Before taking any measures for the betterment of conditions, data regarding availability of parking space, extent of its usage and parking demand is essential. It is also required to estimate the parking fares also. Parking surveys are intended to provide all these information. Since the duration of parking varies with different vehicles, several statistics are used to access the parking need.

- a.** Parking statistics Parking accumulation: It is needed as the number of vehicles parked at a given instant of time. Normally this is expressed by accumulation curve. Accumulation curve is the graph obtained by plotting the number of bays occupied with respect to time.
- b.** Parking volume: Parking volume is the total number of vehicles parked at a given duration of time. This does not account for repetition of vehicles.
- c.** Parking load :
Parking load gives the area under the accumulation curve. It can also be obtained by simply multiplying the number of vehicles occupying the parking area at each time interval with the time interval. It is expressed as vehicle hours.
- d.** Average parking duration: It is the ratio of total vehicle hours to the number of vehicles parked.
- e.** Parking turnover: It is the ratio of number of vehicles parked in a duration to the number of parking bays available. This can be expressed as number of vehicles per bay per time duration.

- f. **Parking index:** Parking index is also called occupancy or efficiency. It is needed as the ratio of number of bays occupied in a time duration to the total space available. It gives an aggregate measure of how effectively the parking space is utilized. Parking index can be found out as follows

$$\text{parking index} = \frac{\text{parking load}}{\text{parking capacity}} = 100 \dots \dots \dots (2 - 1)$$

2.5 Parking Surveys .

Parking surveys are conducted to collect the above said parking statistics. The most common parking surveys conducted are in-out survey, fixed period sampling and license plate method of survey.

1. **In-out survey:** In this survey, the occupancy count in the selected parking lot is taken at the beginning. Then the number of vehicles that enter the parking lot for a particular time interval is counted. The number of vehicles that leave the parking lot is also taken. The final occupancy in the parking lot is also taken. Here the labor required is very less. Only one person may be enough. But we wont get any data regarding the time duration for which a particular vehicle used that parking lot. Parking duration and turn over is not obtained. Hence we cannot estimate the parking fare from this survey.
2. **Fixed period sampling:** This is almost similar to in-out survey. All vehicles are counted at the beginning of the survey. Then after a fixed time interval that may vary between 15 minutes to 1 hour, the count is again taken. Here there are chances of missing the number of vehicles that were parked for a short duration.
3. **License plate method of survey:** This results in the most accurate and realistic data. In this case of survey, every parking stall is monitored at a continuous interval of 15 minutes or so and the license plate number is noted down. This will give the data regarding the duration for which a particular vehicle was using the parking bay. This will help in calculating the fare because fare is estimated based on the duration for which the vehicle was parked. If the time interval is shorter, then there are less chances of missing short-term parkers. But this method is very labor intensive.

2.6 Parking Maximum .

In order to control the amount of land area allocated to parking, a number of cities have established parking maximums. Parking maximums are structured similarly to parking minimums, except that they define the maximum number of parking spaces on per unit basis that a development can provide. Developers may provide fewer parking spaces than the maximum allowed. In San Francisco, California, parking minimums exist for a number of the city's zoning districts, but maximum parking requirements are enforced in the downtown residential district (DTR). The minimum requirements of other areas serve as the maximum amount of street parking that may be provided in the DTR district.

2.7 Method of Determining the Provision of Parking Spaces .

To determine the number of parking spaces that can be used several methods as follows :-

2.7.1 The Method Focuses on the Number of Trips by Car .

This method is applied in the United States, with a coefficient of parking spaces (P), which is searched in the proportion of trips by private vehicle to the total trip vehicles. The amount of the trip is considered very closely related to the number of residents in the region. Magnitude of the coefficient of parking spaces can be formulated as follows .

$$P = \frac{drsc}{oc} \dots\dots\dots (2-2)$$

in which :

- p = coefficient of parking spaces
- d = comparison trip traffic seen at the center city of the hours 7:00 to 19:00 (taken 0.7).
- o = occupancy vehicle (1.5 people / vehicle)
- e = efficiency of the use of parking spaces (taken 0.85)
- r = percentage of vehicles parked at the top of the volume daily (taken 0.25 for a small town and 0.4 for urban large)
- s = peak factor for a moment (take 1.0)
- c = factor that reflects the location of parking needs in the core of the central region

The advantages of this method is that it can determine unknown coefficients for the required parking spaces by looking at the way traffic on downtown and includes a peak hour factor. But the shortcomings of this method is the movement in the city that were

reviewed did not specifically shown, as there is movement of commuters and other traffic movements e.g. trade, and also it is too broad if it will review an area.

2.7.2 The Method Focuses on the Capacity Road-Related Activity Center :-

The number of parking spaces (p) can be expressed as follows:

$$p = \frac{2 \cdot c \cdot k}{100} \dots\dots\dots(2-3)$$

description :

- p = The parking demand
- c = The Capacity of road
- k = The rate of non pass through capacity

The advantages of this method is that it can be determined the need for parking spaces based on the capacity of existing roads, while the downside is if the current through the small streets, the design of existing parking facilities is inefficient or wasteful.

2.7.3 The Method Focuses on the Number of Vehicle Ownership .

In this method, it appears that the higher the number of resident, the smaller the percentage of parking spaces. This method is incompatible with the previous method. The method shows that the larger the population, the higher the percentage of required parking spaces.

The advantage of this method is that it can be show number of vehicles parked on the data of vehicle plate number of the city. But, the shortcoming of this method is if the area under review having visitors or user of the parking lot comes not from the city. In addition, this method is not suitably applied in Indonesia because Indonesia has a large population with higher vehicle ownership.

2.7.4 Method of Volume Cordon Studies with a Maximum Accumulation .

Cordon is an imaginary boundary in the filed of study. It is intended to determine the boundaries of a Central Business District (CBD) area. Studies in cordon volume will use the data of traffic flow on the entire CBD area. To know the maximum accumulation in the cordon, the number of parking space demand can be obtained by the formula as follows.

$$p = A_{i-1} + Li - Oi \dots\dots\dots(2-4)$$

Where :

p = the number of demand for parking spaces

A_{i-1} = accumulated in the previous period (vehicle)

Li = the arrival of vehicles during the period i

Oi = departure of vehicles in the period i

The number of vehicles parking on the cordon can be determined by subtracting the maximum accumulated amount of the number of vehicles with the moving ones within the cordon , as given by the following formula :

$$PV = A - MV \dots\dots\dots(2-5)$$

In which :

PV = estimate of parked cars (vehicles)

A = the maximum accumulation (vehicle)

MV = estimate of a moving car

Meanwhile, the number of moving cars (MV) is estimated by doing a survey. The survey is conducted the traffic on the sections around the area study. The advantage of this method is the flexibility in determining the cordon in study area. As a result, this method can determine which area that has similar land use characteristics. This method is used by conducting a full traffic counting directly in the field that covers the whole movement in a predetermined cordon, either incoming or outgoing. It is expected to know the number of vehicles that do not move (parking) in the region. Afterwards, the need for parking facilities in the region can be identified.

2.8 Effects Of Parking .

Parking has some ill-effects like congestion, accidents, pollution, obstruction to fire-fighting operations etc.

- a. Congestion: Parking takes considerable street space leading to the lowering of

- b. subsequently increase. The operational cost of the vehicle increases leading to great economical loss to the community.
- c. Accidents: Careless maneuvering of parking and unparking leads to accidents which are referred to as parking accidents. Common type of parking accidents occur while driving out a car from the parking area, careless opening of the doors of parked cars, and while bringing in the vehicle to the parking lot for parking.
- d. Environmental pollution: They also cause pollution to the environment because stopping and starting of vehicles while parking and unparking results in noise and fumes. They also affect the aesthetic beauty of the buildings because cars parked at every available space creates a feeling that building rises from a plinth of cars.
- e. Obstruction to fire-fighting operations: Parked vehicles may obstruct the movement of fire-fighting vehicles. Sometimes they block access to hydrants and access to buildings.

2.9 Parking Requirements .

There are some minimum parking requirements for different types of building. For residential plot area less than 300 sq.m require only community parking space. For residential plot area from 500 to 1000 sq.m, minimum one-fourth of the open area should be reserved for parking. may require at least one space for every 70 sq.m as parking area. One parking space is enough for 10 seats in a restaurant where as theatres and cinema halls need to keep only 1 parking space for 20 seats. Thus, the parking requirements are different for different land use zones.

2.10 On Street Parking .

On street parking means the vehicles are parked on the sides of the street itself. This will be usually controlled by government agencies itself. Common types of on-street parking are as listed below. This classification is based on the angle in which the vehicles are parked with respect to the road alignment. As per IRC the standard dimensions of a car is taken as 5, 2.5 meters and that for a truck is 3.75, 7.5 meters.

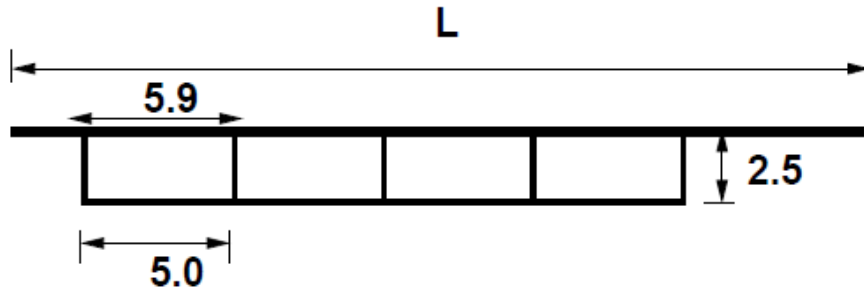


Figure 2-2: Illustration Of Parallel Parking

Parallel parking: The vehicles are parked along the length of the road. Here there is no backward movement involved while parking or unparking the vehicle. Hence, it is the most safest parking from the accident perspective. However, it consumes the maximum curb length and therefore only a minimum number of vehicles can be parked for a given kerb length. This method of parking produces least obstruction to the on-going traffic on the road since least road width is used. Parallel parking of cars is shown in Figure(2-2).

The length available to park N number of vehicles.

$$L = \frac{N}{5.9} \dots\dots\dots(2-6)$$

30° parking: In thirty degree parking, the vehicles are parked at 30_ with respect to the road alignment. In this case, more vehicles can be parked compared to parallel parking. Also there is better maneuverability.

Delay caused to the truck is also minimum in this type of parking. An example is shown in Figure 2-2. From the figure,

$$N = \frac{L}{3.41} - 1.25$$

$$L = 3.41 N + 1.25 \dots\dots\dots(2-7)$$

45°parking: As the angle of parking increases, more number of vehicles can be parked. Hence compared to parallel parking and thirty degree parking, more number of vehicles can be accommodated in this type of parking. From figure 2:3, length of parking space available for parking N number of vehicles in a given kerb is :-

$$L = 3.54 N + 1.77 \dots\dots\dots(2-8)$$

$$N = \frac{L}{3.54} - 1.77$$

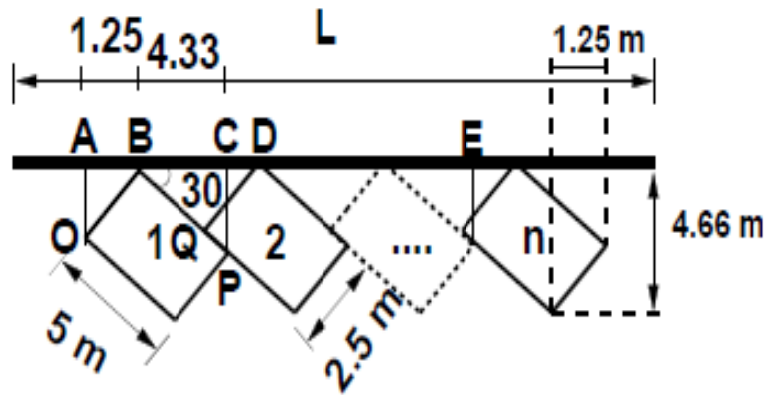


Figure 2-3: Illustration Of 30° Parking

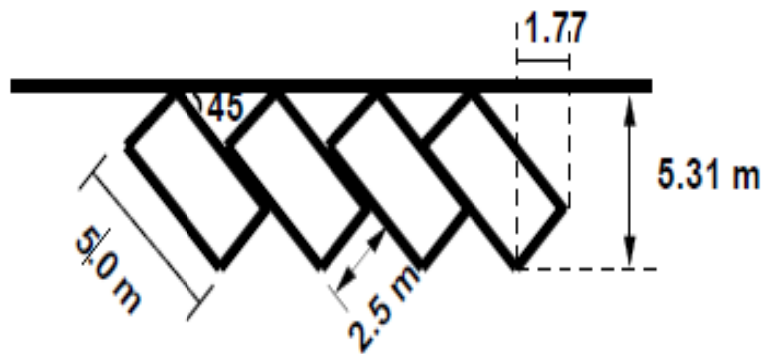


Figure 2-4: Illustration of 45° parking

60° parking: The vehicles are parked at 60° to the direction of road. More number of vehicles can be accommodated in this parking type. From the Figure (2- 4), length available for parking N vehicles = $2.89N+2.16$.

Right angle parking: In right angle parking or 90° parking, the vehicles are parked perpendicular to the direction of the road. Although it consumes maximum width kerb length required is very little. In this type of parking, the vehicles need complex maneuvering and this may cause severe accidents. This arrangement causes obstruction to the road traffic particularly if the road width is less.

However, it can accommodate maximum number of vehicles for a given kerb length. An example is shown in Figure (2 – 5). Length available for parking N number of vehicles is :-

$$L = 2.5N \dots\dots\dots(2-9)$$

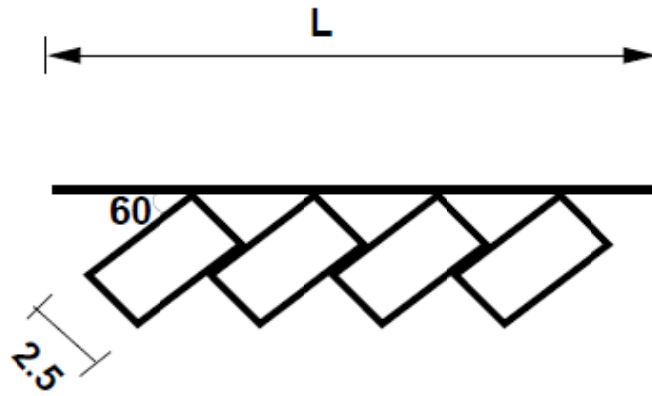


Figure 2-5: Illustration of 60° parking

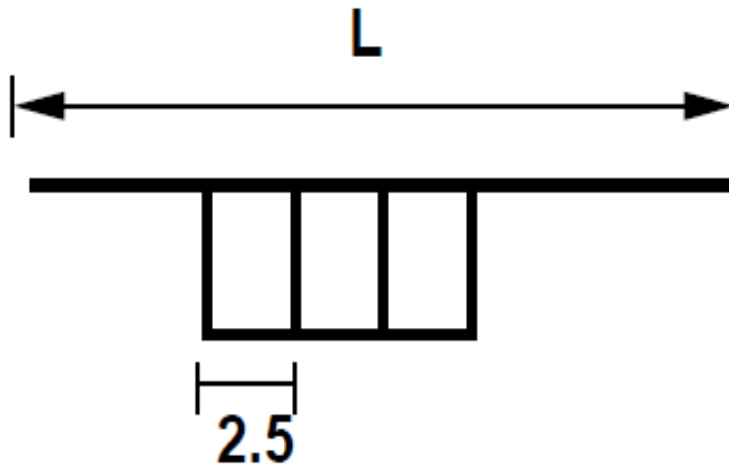


Figure 2-6: Illustration of 90° parking

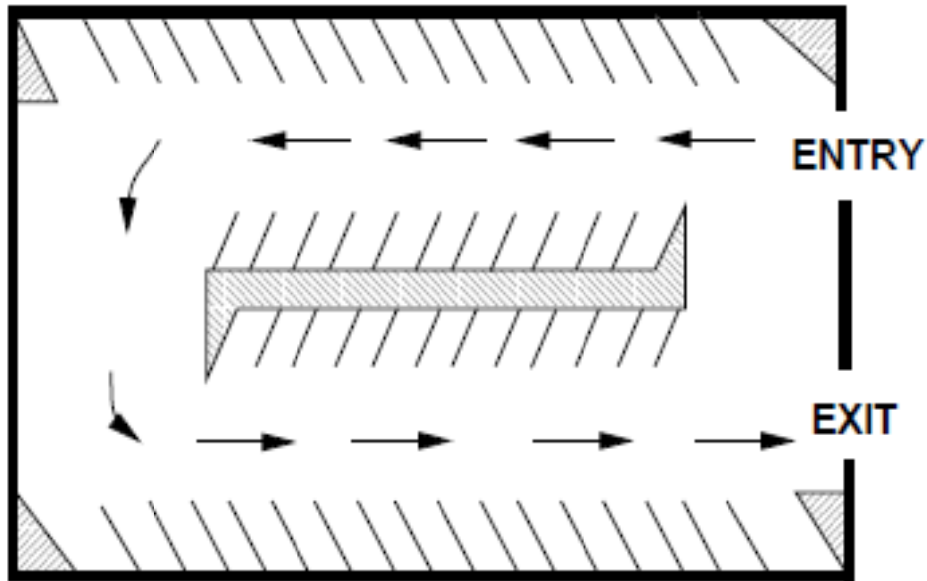


Figure 2-7: Illustration of off-street parking

2.11 Off Street Parking .

In many urban centers, some areas are exclusively allotted for parking which will be at some distance away from the main stream of traffic. Such a parking is referred to as off-street parking. They may be operated by either public agencies or private firms. A typical layout of an off-street parking is shown in figure (2 – 7).

2.12 Parking Lot Design .

2.12.1 General

The parking lot design objective is to maximize the total number of parking spaces in the space available with the following considerations: The parking layout should provide continuous flow of traffic through the lot. The design should allow safe movement of pedestrians from parking to buildings. The design should allow for appropriate landscaping of the parking areas without conflicting with site lighting.

CHAPTER III RESEARCH METHODOLOGY

3.1 Introduction

This chapter discusses the methodology used in this research. The methodology in this research includes research preparation and literature review , data collection ,analysis of the data and conclusion, as shown in the following flow chart . The description of each step is presented in subsequent sections .

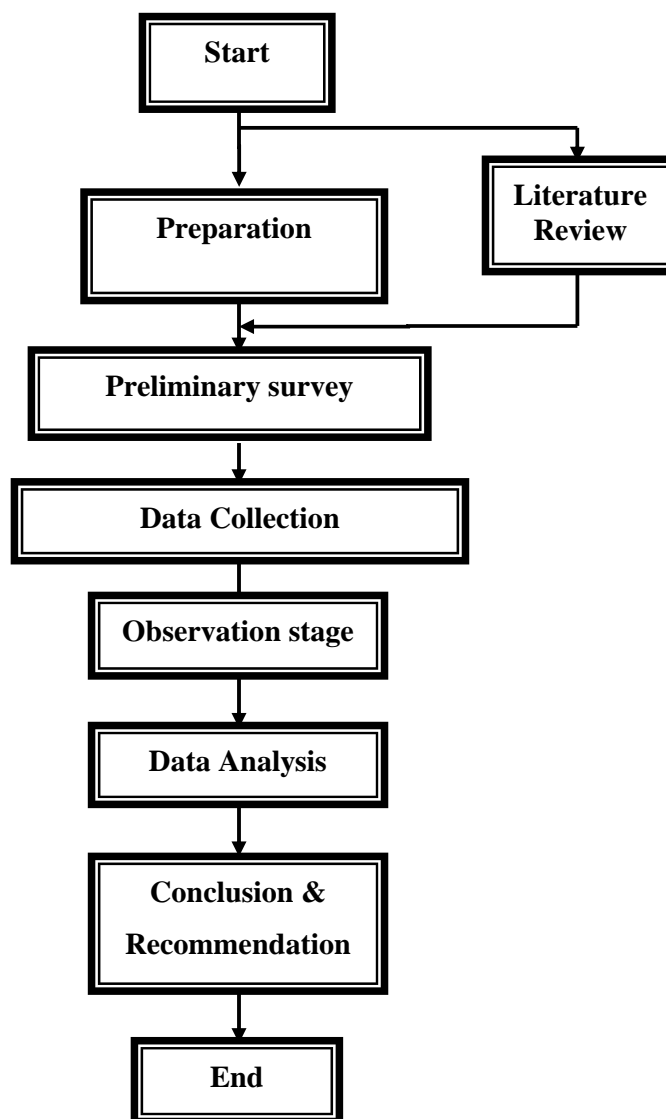


Figure 3.1 Methodology of The Study

Data needed for the analysis are obtained by performance primary survey at the airport, that is meaning through survey to obtain traffic data of vehicles inside Tripoli International Airport parking by the surveyors, and including it practical and parking is random, although planning Parking is the current as a vertical angle (90 degrees). After the survey is completed within the parking and data collection, we can evaluate the existing parking based on data analysis and conclude the extent we need to redesign the situation.

3.2 Types of Data Needed

What is the overall impact on the passenger terminal as a whole?

To answer all those questions, some primary data is taken by interview to the related authority of the Tripoli international airport, those are vice president of planning and general manager the question should consist of the following:

- a. Operational activity of the airport.
- b. airport future development planning, especially of parking lots parking system in the future.

The secondary data is taken by the official report of Tripoli international airport that consist of data such as

- a. the number of cars entry and exit in parking.

Data needs in this research include primary data, such as traffic data, and secondary data.

In this study, the material from the primary data is data about the number of vehicular traffic and persons coming from and headed to the airport.

- The number of vehicular traffic.
- Amount of traffic to be involving many vehicles coming to (attracted traffic) and go from (generated traffic) airport per unit time (minutes or hours), vehicle type, occupancy levels, etc.
- Parking of area of Tripoli International Airport is located between the main gate (entry & exit) even of the side of road opposite the terminal building and estimated the total parking of area 25,000 square meters approximately.

3.3 Parking Data Collection Methods

There are many methods that can be used to measure time of incoming and outgoing Vehicle Cycles. because it requires large resources. survey process this survey snapped data per vehicle ie vehicle number and the time of entry of the position as well as the time to go out and be over the whole day

3.4 Time To Collect Secondary .

- a. Data is collected secondary traffic passenger vehicles in Sunday because it first work day at week and the weekend days are Friday – Saturday. We start traffic survey at 7 am till even 7 pm through .
traffic survey for parking existing monitors the number passenger cars entering the parking lot the airport and beyond to prepare spreadsheets for each period quarter of an hour (per 15 min.), and it clear to us the number of passenger cars of all times.
- b. We also need to know the number of passengers arriving and departing each month in order to conclude the process of parking arena where it is limited to the number are cars and duration of their stay position during weekdays and holidays.

3.5 Parking System At The Tripoli International Airport Is To The Following .

1. There is existing situation is a major gateway and one way is to cross passenger cars into the parking lot.
2. There are major exit gate and deduction of the financial value of parking fees and we can see how many emerging and remaining within the car park.
3. There is a special position public buses outside the station own parking space at the airport and cause severe congestion and irregular traffic flow as a result of the overlapping of passenger cars with buses.

3.6 Supply Data .

supply data of Tripoli International Airport parking are: -

1. The number of rows of parking position 7 rows and each row accommodate 40 vehicles in addition to grades sides and 120 vehicles bladder However, there are some random vehicles parking .

2. Planning is currently available for parking is at a 90-degree angle as shown in Figure (1-6) described previously.
3. Note that there are some vehicles outside the airport parking lot, as shown in Figure (1-6).
4. Based on traffic data field for vehicles within the car park which will we get them in the coming days is to know the desired value of the number of vehicles that exceed the capacity of the car park currently exists and which conclude amplitude required depending on the results of data analysis vehicular traffic within the parking allocated and addition to the number of vehicles which are in the random positions.

CHAPTER IV

RESULTS AND ANALYSIS

4.1 Data Collection

Data collected in the field is the traffic data and parking. Traffic data collection using the traffic counting method performed at each entrance and exit of the parking main gate . The capacity of cars parking area is 378 cars .

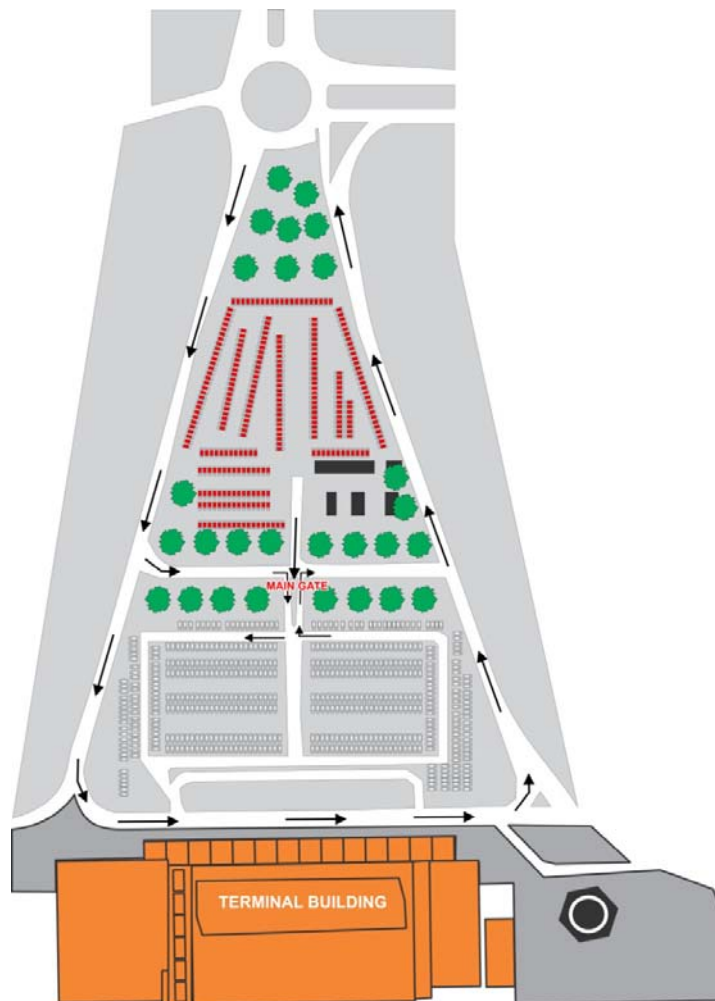


Figure 4.1 land use of parking

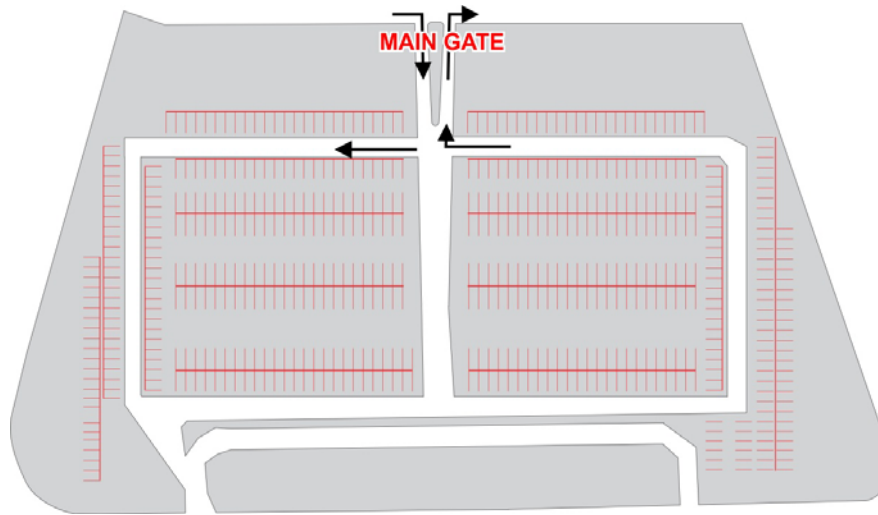


Figure 4.2 Existing parking space

The survey conducted during one day at 21 of January 2013 for 12 hours Starting from 7:00 am until 7:00 pm , the survey period was divided 48 period to the traffic patterns of parking Tripoli International Airport ,Surveyed by recording the number of cars for one day. The duration of each period of 15 minutes. The result of the survey can be seen on following Table (4-1).

Table. 4.1 Number of Vehicles Arrived Hourly

Arrival Time (h)	Number of Vehicles
7 : 00 – 7 : 59	263
8 : 00 – 8 : 59	217
9 : 00 – 9 : 59	297
10:00 – 10 : 59	185
11 : 00 – 11 : 59	123
12 : 00 – 12 : 59	96
13 : 00 – 13 : 59	255
14 : 00 – 14 : 59	191
15 : 00 – 15 : 59	77
16 : 00 – 16 : 59	56
17 : 00 – 17 : 59	94
18 : 00 – 18 : 59	177
Total	2031

Table. 4.2 Number of Vehicles Departed Hourly

Departed Time (h)	Number of Vehicles
7 : 00 – 7 : 59	81
8 : 00 – 8 : 59	142
9 : 00 – 9 : 59	219
10:00 – 10 : 59	251
11 : 00 – 11 : 59	187
12 : 00 – 12 : 59	121
13 : 00 – 13 : 59	152
14 : 00 – 14 : 59	210
15 : 00 – 15 : 59	147
16 : 00 – 16 : 59	94
17 : 00 – 17 : 59	66
18: 00 – 18 : 59	194
Total	1864

From the table and figure above, it appears that the activities of parking , on the average, has a level of crowded of nearly similar density at almost all of the time of observation since 7:00 am to 10:00 am , except the period of 11:00 am to 1 pm and 2:00 pm to 7:00 pm when it somewhat reduces. According to the vehicle arrival data, it can be said that parking activities is already at the highest level if there are many trips in one time. One characteristic of parked vehicles that should be known is parking duration. To do so, it is necessary to count on the number of vehicles parked for various durations. In this case, the analysis was conducted on 10-minute basis. After 60-minute parking duration, the duration category was increased since the number of parked vehicles decreased by increasing parking duration.

Table 4.3 the Number of Vehicles Parked based on Each Parking Time

Parking Duration (minutes)	Number of Cars	%
<0:10	324	17%
0:10-0:19	129	7%
0:20-0:29	182	10%
0:30-0:39	154	8%
0:40 - 0:49	229	12%
0:50-0:59	145	8%
1:00-1:29	318	17%
1:30-1:59	232	13%
2:00-2:59	151	8%
	1864	100%

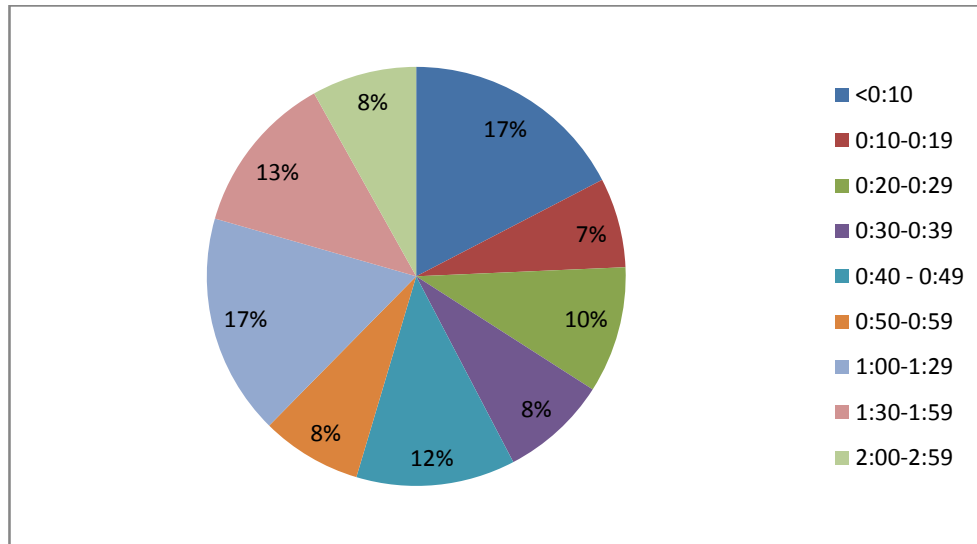


Figure 4.5. Number Parked based on Each Parking Time of Vehicles

Based on Tables (A-1) attached in appendix note that survey of traffic data collection for Tripoli International Airport parking has start from 7 am to 7 pm where they were 4 surveyors relying on numbers plates of vehicles and also the time of arrival and departure of vehicles for parking and that we have obtained the total of parking time for all vehicles within the park equal to 96,659 minutes during the specified period of the survey, so that value will be get the average of parking time as indicated in the following equation :-

$$AV = \frac{\sum Duration\ time}{N} \dots\dots\dots(4.1)$$

$$AV = \frac{96.659}{1864} = 52\ minutes$$

Based on the table above, it seems that most visitors have a short visit, as evidenced by the percentage of cars parked less than 10 minutes, and that is equal to 17%. This is apparently because it is already had planned in advance for the survival of the airport arrivals. So most of the cars were parked under the average time, parked approximately 42% of cars less than 40 minutes, the average of parking period 52 minutes unfortunately, some cars parked for a long time.

For example, there is a parked car 38% more than 1 hour .it will contribute to reducing the number of parking spaces for a long time could exceed several days and that is something which causes severe congestion problem and the lack of parking spaces. The following is the table of the accumulation of vehicles parked. To make a simple and clear calculation, the number of vehicles arrived, the number of vehicles departed and the accumulation is presented in 1-hour time period basis.

4.2 Demand of parking

The demand of parking can be determined by calculating the accumulation of parking time, by using volume cordon studies (Shane and Roess, 1990), given by:.

$$P = A_{i-1} + L_i - O_i$$

In which:

P = the number of demand for parking spaces

A_{i-1} = accumulated in the previous period (vehicles)

L_i = the arrival of vehicles during the period i

O_i = departure of vehicles in the period i, so the calculation it will be as shown in the following:-

$$A_{-1} = 87$$

$$L_i = 864$$

$$O_i = 442$$

$$P = 87+864-442 =509$$

Table 4.4. The Accumulation of Vehicles Parked

Time Period	Number of Vehicle Arrived	Accumulation of arrived/ veh. Li	Number of Vehicle Departured	Accumulation of dep./veh. Oi	Accumulation
Before 7:00	87	87	-	-	-
7 : 00 – 7 : 59	263	350	81	81	269
8 : 00 – 8 : 59	217	567	142	223	344
9 : 00 – 9 : 59	297	864	219	442	422
10:00 – 10 : 59	185	1049	251	693	356
11 : 00 – 11 : 59	123	1172	187	880	292
12 : 00 – 12 : 59	96	1268	121	1001	267
13 : 00 – 13 : 59	255	1523	152	1153	370
14 : 00 – 14 : 59	191	1714	210	1363	351
15 : 00 – 15 : 59	77	1791	147	1510	281
16 : 00 – 16 : 59	56	1847	94	1604	243
17 : 00 – 17 : 59	94	1941	66	1670	271
18 : 00 – 18 : 59	177	2118	194	1864	254

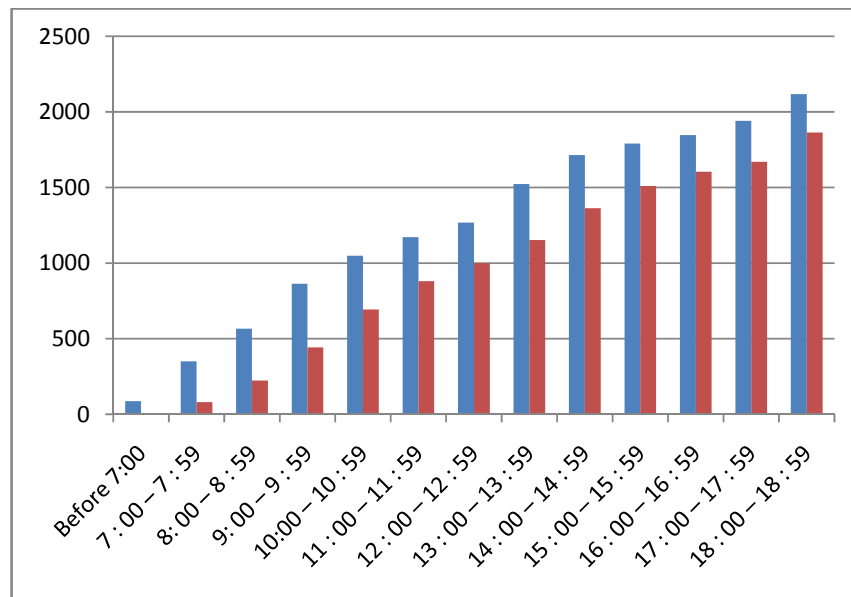


Figure 4.6 Accumulation of arrived & departure vehicle

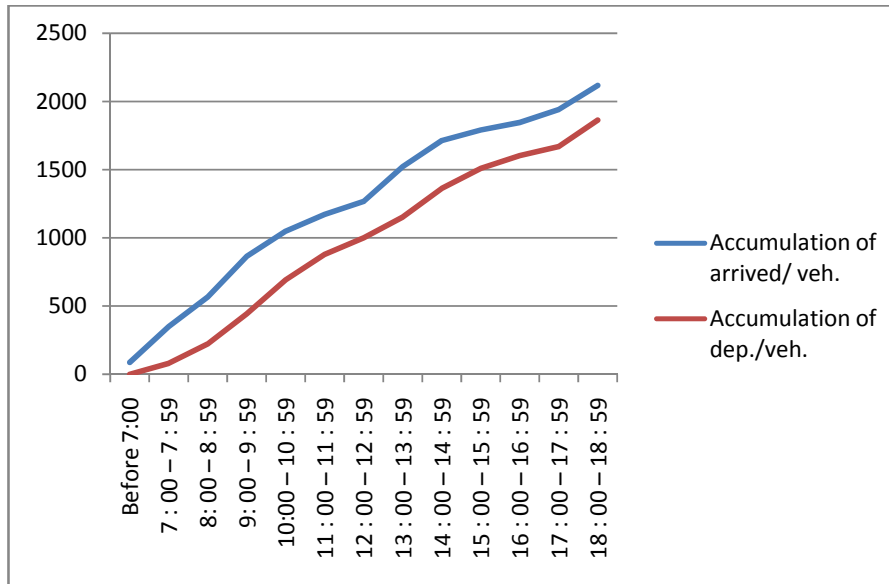


Figure 4.7. The Line Graph of the Accumulation of Vehicles Parked

4.3 Use of Another Parking System

As mentioned in chapter 2, there are several parking system that can be selected, and then by using Litman formula, it can be calculated the number of parking space of each parking types to evaluate its suitability against the parking demand of airport.

Use (L) the total of curb length for this equation it will be obtain from existing parking , so it is equal 1254 m .

- a. 30⁰ -Angular Parking System

In this system, the number of parking space is:

$$N = \frac{L-1,25}{3,41}$$

$$N = \frac{1254-1,25}{3,41} = 367 \text{ lot}$$

- b. 45⁰ -Angular parking system

In this system, the number of parking space is:

$$N = \frac{L-1,77}{3,54}$$

$$N = \frac{1254-1,77}{3,54} = 354 \text{ lot}$$

- c. 60⁰ -Angular parking system

In this system, the number of parking space is:

$$N = \frac{L-2,16}{2,89}$$

$$N = \frac{1254-2,16}{2,89} = 433 \text{ lot}$$

d. 90° -Angular parking system

In this system, the number of parking space is:

$$N = \frac{L}{2,50}$$

$$N = \frac{1254}{2,50} = 502 \text{ lot}$$

4.4 Creating Rules of Parking Time Allowed

According to Table 4.3, the average parking time is 52 minutes. However, there were 38 % vehicles parked more than 1 hour. This certainly will decrease the number of parking spaces. Therefore, to give sufficient number of parking spaces and to anticipate of increasingly parking activities, it is proposed to create a better rule of parking time allowed. So that, if the average parking time reduces, the maximum accumulation of vehicles parked is expected to reduce.

The optimal parking capacity can be calculated by using the following equation.

A = Cumulative parking activity on one day (12 hours)

B = Optimal parking capacity

$$B = \frac{\text{Activity time}}{\text{average duration time}} * \text{Number of parking spaces}$$

A - Optimal parking capacity for 30° -angular parking system:

Activity time = 12 hour

Average duration time = 52 min.

Number of parking space = 245

$$B = \frac{12}{52/60} * 245 = 3393$$

The optimal parking capacity is more than cumulative parking activities in one day, therefore, this parking system is recommended.

B - Optimal parking capacity for 45° -angular parking system:

Activity time = 12 hour

Average duration time = 52 min.

Number of parking space = 354

$$B = \frac{12}{52/60} * 354 = 4901$$

C - Optimal parking capacity for 60°-angular parking system:

Activity time = 12 hour

Average duration time = 52 min.

Number of parking space = 426

$$B = \text{---} * 426 = 5899$$

D - Optimal parking capacity for 90°-angular parking system:

Activity time = 12 hour

Average duration time = 52 min.

Number of parking space = 502

$$B = \text{---} * 502 = 6951$$

The optimal parking capacity is 6924, it is more than cumulative parking activities incoming in one day 2118 and therefore, this parking system is recommended.

The following figure shows the optimal parking layout for 90°-angular parking system.

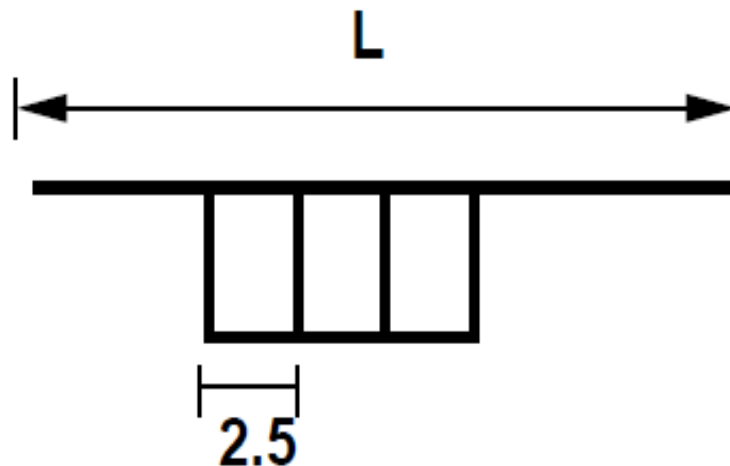


Figure 4.8 Optimal Parking Layout for 90°-angular Parking System

It can be said clearly that among these three parking systems, parking system, 90°-angular able to provide more lots for the parking demand .

Conclude from the foregoing traffic data analysis of vehicular within the Tripoli International Airport parking and after comparison the results and obtained, find the demand number of parking space which is equal 509 lots and after used to the four methods for different parking system angular 30° , 45° , 60° , 90° , through analysis of the data for number of parking space found the largest number of parking spaces system is achieved by using a 90° which is same the existing planning now.

Main problem it does not provide the demand number parking space but the problem is the lack of coordination and organization vehicles and parked properly in places allocated to them as shown in Figure 1-6 at chapter I in the problem statement paragraph and therefore we can say that problem is the problem of regulating the conduct Parking meaning attributed the problem to basic mismanagement over the car park and also the behavior of driving where they are parking indiscriminately incorrectly and this is something that requires a review of the management process position passenger terminal and also to redesign the terminal building parking have to re-planned angle of 90° in order to provide a larger number of parking space thus increase the capacity of the current terminal building parking , but make planning based on completion of a new terminal buildings on sides .

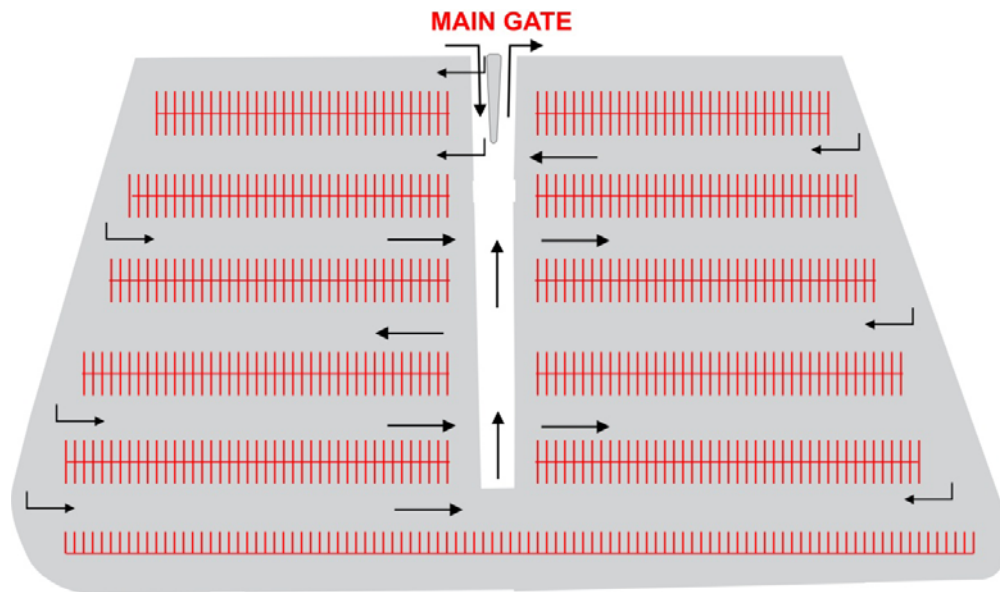


Figure 4.9 New parking design

CHAPTER V

CONCLUSIONS AND RECOMMENDATIONS

5.1 Conclusions

Proposal of this study is a redesign of parking Tripoli International Airport, and evaluation the performance In the existing parking 90° as shown in chapter I cause of this problem , Of the problem statement that is causing the lack of capacity for terminal parking and also there is no place dedicated to public transport .

so we've compiled all the data and required details for analysis and through the data collected from number of vehicles within parked, we have to conduct the survey as described in data tables provide in (A-1), it can observe the peak hour time at morning (9:00-10:00) as shown in chart for arrival time, so the analysis of this data by using equations previously described chapter II where the data were analyzed and getting calculating the optimum capacity of the parking regulations.

There are many parking systems to apply and get the optimum capacity of parking, these systems are represent as:-

1. 30° angular parking system.
2. 45° angular parking system.
3. 60° angular parking system.

The number of parking space for all these systems as explained in chapter IV, We conclude from analysis of the data and the calculation and the number of spaces Parking system find 90° angular provides 502 lots.

That is mean largest number of parking spaces must be use where, to calculate the optimum capacity for this value to number of parking space .It is noted that the parking demand and the number of parking spaces differ only slightly, and the average parking time is short enough at 52 minutes. If for one reason or another, the number of vehicles arrived or the average of vehicles parking time increase, this situation can also increase the parking demand.

When determine the average of parking time (52 min.) at whole survey day. The maximum value of parking capacity taken in 90° angular parking system 6924.

5.2 Recommendations

After previous conclusions and see the results of data analysis, where it was a peak hour start from (9 am to 10 am) which is cause of the congestion in Tripoli international airport parking and irregular parking system, found insufficient of parking capacity during this period which need to solve this problem so recommend to re-design Parking Tripoli International Airport to provide all facilities necessary needed of parking and must make special parking for public transport also places allocated for people with special needs and need to increase the capacity of terminal parking meet the needs of the delegates to the airport as well as taking into account the new terminals, which is under construction now.

Further increase the parking capacity, in anticipation of the increasing number of vehicles to be parked in the future. This change will increase the number of parking spaces for more vehicles than the current parking demand.

New rules of parking which charge a significant additional parking when a vehicle is parked longer than 1 hour should be implemented. If the average parking time can be reduced, the maximum accumulation of vehicles parked will also be reduced to improve traffic further.

Has collected the data by using four persons and supervisor engineer in charge of the collection of traffic data vehicles saluting the two persons collecting the data as number of cars and plate number at the main gate (entry and exit) also completed the survey of traffic data and organize parking inside position the other two persons and the control engineer in charge of the airport authority.

Depending on the results of data collection are described in the accompanying tables below these data are analyzed according to the method of analysis used it this Tables (4.1,4.66) Car survey .

REFERENCES

- Paul C. Box ,joseph C. oppenlander, ph.D.manual of trafic engineering studies , fotrth edition (1976)
- John D. Edwards jr., P.E. Transportation planning hand book .1992 by PTP printice-Hall, Inc
- Berry, d.s., (1990). The technology of urban transportation, north western university press, USA. Catanese, a. J. & James c. Snyder, (1979). Introduction to urban planning, mc Graw Hill book company, new york, USA.
- Dickey, j. W., (1983). Metropolitan transportation planning: second edition, mc Graw Hill book company, washington, USA.
- Institute of transportation engineers, (1994). Engineerig Manual of transportation studies, Prentice Hall, Englewood Cliffs, New Jersey, USA.
- Institute of traffic engineers, (1994). An introduction to highway transportation engineering, washington d. C., USA.
- Transportation Engineering 38.1 Tom V. Mathew and K V Krishna Rao .
- Tamin, o. Z., (2002). Traffic impact analysis, PWK UNDIP journals, publishers UNDIP, Semarang.
- Dickey, j. W., (1983). Metropolitan transportation planning: second edition, mc Graw Hill book company, washington, USA.
- Institute of traffic engineers, (1994). An introduction to highway transportation engineering, washington d. C., USA.
- Sudjana, (1998). Statistical methods, publisher tarsito, bandung.
- Tripoli International airport authority guidelines for planning and operation of the Parking Facilities .

Appendix

A

A-1

Starting Time : 7:00

Survey Date: 21 January 2013

Tabel A.1 Car survey(continuation)

NO	Plate no.	Arrival time	Dep. time	Duration of time
1	29-5-821302 L	7:00	7:05	5 min
2	14-5-212223 L	7:00	7:16	16 min
3	25-8-123567 L	7:00	7:45	45 min
4	12-10-678977 L	7:00	-	-
5	25-25-7865 L	7:01	7:28	27 min
6	1-1-987453 L	7:01	7:08	7 min.
7	1-11-1233 L	7:01	9:02	121 min.
8	7-5-555821 L	7:01	8:39	98 min.
9	8-4-987622 L	7:01	7:24	23 min .
10	3-9-1144889 L	7:02	7:09	7 min.
11	9-7-77644 L	7:02	7:22	20 min
12	7-7-70001 L	7:02	8:05	63 min.
13	1-11-40 L	7:02	7:11	9 min.
14	3-7-189765 L	7:03	8:32	89 min
15	20-5-133512 L	7:03	7:33	30 min
16	24-5-221051 L	7:03	9:15	132 min.
17	28-5-87978 L	7:03	8:55	112 min.
18	25-5-33331 L	7:03	7:10	7 min.
19	15-5-252589 L	7:03	7:53	50 min.
20	12-25-67492 L	7:04	7:12	8 min.
21	13-5-987654 L	7:04	8:47	103 min.
22	12-4-56789 L	7:04	7:12	8 min.
23	3-2-847493 L	7:04	7:10	6 min
24	10-10-56893 L	7:04	8:36	92 min.
25	5-11-123987 L	7:05	7:47	42 min
26	13-11-3322 L	7:05	-	-
27	1-11-2345 L	7:05	7:54	49 min
28	1-11-981255 L	7:05	9:35	150 min.
29	7-8-840912 L	7:05	9:00	115 min.
30	4-28-2834 L	7:05	7:30	25 min
31	12-11-2222 L	7:05	7:13	8 min
32	10-11-56666 L	7:05	7:17	12 min

NO	Plate no.	Arrival time	Dep. Time	Duration
33	2-3-1702 L	7:05	7:26	21 min
34	29-9-212223 L	7:06	7:44	38 min
35	22-12-438491 L	7:06	8:57	111 min
36	12-10-678977 L	7:06	7:11	5 min
37	25-25-7865 L	7:06	-	-
38	1-1-987453 L	7:07	7:22	15 min
39	1-11-1233 L	7:07	7:39	32 min
40	7-5-555821 L	7:08	7:59	51 min.
41	8-4-987622 L	7:08	7:30	22 min
42	4-9-1144889 L	7:08	8:16	68 min
43	4-7-77544 L	7:08	9:15	127 min
44	7-7-789111 L	7:08	7:57	49 min
45	1-11-40 L	7:08	7:39	31 min
46	3-7-189765 L	7:09	7:20	11 min
47	20-5-133512 L	7:09	8:58	109 min
48	24-5-221051 L	7:09	7:53	44 min
49	28-5-87978 L	7:09	7:40	31 min
50	25-5-33331 L	7:09	8:42	93 min
51	23-6-252589 L	7:09	-	-
52	12-25-549120 L	7:10	7:23	13 min
53	14-5-9843267 L	7:10	8:03	53 min.
54	11-4-56789 L	7:10	7:33	23 min
55	13-5-847493 L	7:10	8:24	74 min
56	5-10-56893 L	7:10	7:59	49 min
57	6-4-1234987 L	7:11	9:08	117 min
58	16-11-33122 L	7:11	7:44	33 min
59	21-11-2345 L	7:11	8:07	56 min
60	1-11-987355 L	7:12	9:36	144 min
61	8-8-840912 L	7:12	7:34	22 min
62	3-5-12874 L	7:12	7:41	29 min
63	12-11-222 L	7:12	7:55	43 min
64	10-5-566766 L	7:12	7:18	6 min

NO	Plate no.	Arrival time	Dep. Time	Duration time
65	5-5-55667 L	7:13	7:20	7 min.
66	14-5-765223 L	7:13	7:22	9 min.
67	25-7-496567 L	7:13	8:07	54 min.
68	12-5-675577 L	7:13	9:03	110 min
69	23-25-78465 L	7:13	8:27	74 min.
70	1-1-987453 L	7:14	9:20	126 min
71	1-11-1233 L	7:14	8:00	46 min.
72	7-5-555821 L	7:14	8:06	52 min
73	8-4-987622 L	7:14	9:04	110 min.
74	3-9-1144889 L	7:14	7:22	8 min.
75	9-7-77644 L	7:15	8:18	63 min.
76	7-7-70001 L	7:15	8:47	92 min
77	1-11-40 L	7:15	7:22	7 min.
78	3-7-189765 L	7:16	7:41	25 min.
79	20-5-133512 L	7:16	9:08	112 min
80	24-5-221051 L	7:16	7:34	18 min
81	28-5-87978 L	7:16	8:39	83 min.
82	25-5-33331 L	7:16	8:10	54 min
83	15-5-252589 L	7:17	8:19	62 min.
84	12-25-67492 L	7:17	7:36	19 min
85	13-5-987654 L	7:17	7:22	5 min.
86	12-4-56789 L	7:17	9:18	121 min
87	3-2-847493 L	7:17	7:24	7 min.
88	10-10-56893 L	7:18	8:17	59 min
89	5-11-123987 L	7:18	7:35	17 min.
90	13-11-3322 L	7:18	9:02	104 min
91	1-11-2345 L	7:19	7:57	38 min
92	1-11-981255 L	7:19	8:34	75 min.
93	7-8-844912 L	7:19	-	-
94	4-28-2834 L	7:19	9:48	149 min
95	12-11-2222 L	7:19	8:06	47 min.
96	1-11-5666 L	7:20	7:53	33 min

NO	Plate no.	Arrival time	Dep. Time	Duration
97	29-5-821302 L	7:20	8:01	41 min.
98	14-5-212223 L	7:20	7:27	7 min.
99	25-8-123567 L	7:20	9:33	133 min
100	12-10-678977 L	7:20	8:29	69 min.
101	25-25-7865 L	7:21	-	-
102	1-1-987453 L	7:21	8:50	89 min.
103	1-11-1233 L	7:21	7:56	35 min
104	7-5-555821 L	7:21	7:30	9 min.
105	8-4-987622 L	7:21	8:15	54 min.
106	3-9-1144889 L	7:22	9:04	102 min.
107	9-7-77644 L	7:22	8:05	43 min
108	7-7-70001 L	7:22	9:00	98 min
109	1-11-40 L	7:22	7:27	5 min.
110	3-7-189765 L	7:22	8:17	55 min.
111	20-5-133512 L	7:22	7:54	32 min
112	24-5-22551 L	7:23	-	-
113	28-5-87578 L	7:23	7:46	23 min
114	25-5-3351 L	7:23	7:34	11 min
115	15-5-25489 L	7:23	7:31	8 min.
116	12-25-67492 L	7:23	10:11	168 min.
117	13-5-987654 L	7:23	7:55	32 min
118	12-4-56789 L	7:23	8:10	47 min.
119	3-2-847493 L	7:23	8:03	40 min
120	10-10-56893 L	7:23	7:30	7 min.
121	5-11-123987 L	7:24	7:54	30 min.
122	13-11-3322 L	7:24	7:33	9 min.
123	1-11-2345 L	7:24	7:46	22 min
124	1-11-981255 L	7:24	8:21	57 min
125	7-8-840912 L	7:24	9:46	142 min.
126	4-28-2834 L	7:24	8:37	73 min.
127	12-11-422 L	7:25	7:30	5 min.
128	2-10-6436 L	7:25	7:54	29 min.

NO	Plate no.	Arrival time	Dep. Time	Duration
129	29-5-821302 L	7:25	7:34	9 min.
130	14-5-212223 L	7:25	9:23	118 min.
131	25-8-123567 L	7:25	7:37	12 min.
132	12-10-678977 L	7:26	8:11	45 min.
133	25-25-7865 L	7:26	8:05	39 min.
134	1-1-987453 L	7:26	9:54	148 min.
135	1-11-1233 L	7:26	8:07	41 min
136	7-5-555821 L	7:26	9:02	96 min.
137	8-4-987622 L	7:27	8:02	35 min
138	3-9-1144889 L	7:27	9:15	108 min.
139	9-7-77644 L	7:27	-	-
140	7-7-70001 L	7:28	9:03	95 min.
141	1-11-40 L	7:28	9:21	113 min.
142	3-7-189765 L	7:28	8:10	42 min.
143	20-5-133512 L	7:28	8:20	52 min
144	24-5-221051 L	7:28	9:01	93 min.
145	28-5-87978 L	7:29	8:31	62 min.
146	25-5-33331 L	7:29	7:34	5 min.
147	15-5-252589 L	7:29	8:18	49 min.
148	12-25-67492 L	7:29	9:30	121 min
149	13-5-987654 L	7:29	9:10	101 min.
150	12-4-56789 L	7:30	8:21	51 min
151	3-2-847493 L	7:30	8:12	42 min
152	10-10-56893 L	7:30	9:16	106 min.
153	5-11-123987 L	7:30	7:38	8 min.
154	13-11-3322 L	7:30	8:08	38 min.
155	1-11-2345 L	7:30	9:20	110 min.
156	1-11-981255 L	7:31	7:57	26 min
157	7-8-840912 L	7:31	8:27	56 min.
158	4-28-2834 L	7:31	7:40	9 min.
159	12-11-2222 L	7:31	7:36	5 min.
160	10-11-56666 L	7:32	8:08	36 min

NO	Plate no.	Arrival time	Dep. Time	Duration
161	29-5-821302 L	7:32	9:24	112 min
162	14-5-212223 L	7:33	8:23	50 min
163	25-8-123567 L	7:33	8:13	40 min
164	12-10-678977 L	7:34	9:32	118 min
165	25-25-7865 L	7:34	-	-
166	1-1-987453 L	7:34	9:06	92 min
167	1-11-1233 L	7:34	7:39	5 min
168	7-5-555821 L	7:34	8:26	52 min
169	8-4-987622 L	7:35	8:24	49 min
170	3-9-1144889 L	7:35	9:09	94 min
171	9-7-77644 L	7:35	9:13	98 min
172	7-7-70001 L	7:35	9:48	133 min
173	1-11-40 L	7:36	8:19	43 min
174	3-7-189765 L	7:36	9:26	110 min
175	20-5-133512 L	7:37	8:30	53 min
176	24-5-221051 L	7:37	8:14	37 min
177	28-5-87978 L	7:37	9:19	102 min
178	25-5-33331 L	7:37	9:58	141 min
179	15-5-252589 L	7:37	8:22	45 min
180	12-25-67492 L	7:37	9:16	99 min
181	13-5-987654 L	7:37	-	-
182	12-4-56789 L	7:38	8:33	55 min
183	3-2-847493 L	7:38	8:14	36 min
184	10-10-56893 L	7:38	9:30	112 min
185	5-11-123987 L	7:39	9:10	91 min
186	13-11-3322 L	7:39	8:21	42 min
187	1-11-2345 L	7:39	9:52	133 min
188	1-11-981255 L	7:39	8:23	44 min
189	37-8-840912 L	7:40	9:27	107 min
190	4-28-2834 L	7:40	8:29	49 min
191	12-11-2222 L	7:40	8:31	51 min
192	10-11-56666 L	7:40	9:12	92 min

NO	Plate no.	Arrival time	Dep. Time	Duration
193	29-5-821302 L	7:41	-	-
194	14-5-212223 L	7:41	8:14	33 min
195	25-8-123567 L	7:41	9:51	130 min.
196	12-10-678977 L	7:41	8:57	76 min.
197	25-25-7865 L	7:41	8:02	21 min
198	1-1-987453 L	7:42	8:36	54 min
199	1-11-1233 L	7:42	9:10	88 min.
200	7-5-555821 L	7:42	8:50	8 min.
201	8-4-987622 L	7:42	8:16	34 min
202	3-9-1144889 L	7:43	8:02	19 min
203	9-7-77644 L	7:43	-	-
204	7-7-70001 L	7:43	9:46	123 min
205	1-11-40 L	7:43	8:38	55 min
206	3-7-189765 L	7:43	8:26	43 min.
207	20-5-133512 L	7:44	8:12	28 min
208	24-5-221051 L	7:44	9:00	16 min
209	28-5-87978 L	7:45	8:27	42 min
210	25-5-33331 L	7:45	10:32	167 min
211	15-5-252589 L	7:45	10:06	141 min
212	12-25-67492 L	7:46	8:43	57 min.
213	13-5-987654 L	7:46	7:57	11 min
214	12-4-56789 L	7:46	9:13	87 min.
215	3-2-847493 L	7:46	8:17	31 min
216	10-10-56893 L	7:47	8:14	27 min
217	5-11-123987 L	7:47	8:22	35 min
218	13-11-3322 L	7:47	-	-
219	1-11-2345 L	7:48	8:44	56 min
220	1-11-981255 L	7:48	9:00	72 min.
221	7-8-840912 L	7:48	7:43	5 min
222	4-28-2834 L	7:49	8:02	13 min
223	12-11-2222 L	7:49	8:13	24 min
224	10-11-56666 L	7:50	9:06	66 min.

NO	Plate no.	Arrival time	Dep. Time	Duration
225	29-5-821302 L	7:50	9:19	89 min.
226	14-5-212223 L	7:50	10:25	155 min.
227	25-8-123567 L	7:50	8:06	16 min
228	12-10-678977 L	7:50	7:55	5 min.
229	25-25-7865 L	7:51	-	-
230	1-1-987453 L	7:51	7:58	7 min.
231	1-11-1233 L	7:51	8:40	49 min.
232	7-5-555821 L	7:51	8:00	9 min.
233	8-4-987622 L	7:52	8:19	27 min
234	3-9-1144889 L	7:52	7:58	6 min.
235	9-7-77644 L	7:52	8:56	64 min.
236	7-7-70001 L	7:53	8:01	8 min.
237	1-11-40 L	7:53	8:23	30 min.
238	3-7-189765 L	7:53	9:25	92 min.
239	20-5-133512 L	7:54	8:12	18 min
240	24-5-221051 L	7:54	-	-
241	28-5-87978 L	7:54	-	-
242	25-5-33331 L	7:54	7:59	5 min.
243	15-5-252589 L	7:54	8:45	51 min
244	12-25-67492 L	7:55	8:04	9 min.
245	13-5-987654 L	7:55	10:06	131 min
246	12-4-56789 L	7:55	8:16	21 min
247	3-2-847493 L	7:55	9:27	92 min
248	10-10-56893 L	7:56	8:07	11 min
249	5-11-123987 L	7:56	8:53	57 min
250	13-11-3322 L	7:56	8:42	46 min.
251	1-11-2345 L	7:56	9:21	85 min
252	1-11-981255 L	7:56	9:37	101 min
253	7-8-840912 L	7:56	9:15	79 min.
254	4-28-2834 L	7:56	-	-
255	12-11-2222 L	7:58	9:50	112 min
256	10-11-56666 L	7:58	10:01	123 min.

No	Plate no	Arrival time	Dep. Time	duration
257	29-5-821 L	7:58	8:06	8 min.
258	14-5-212223 L	7:59	8:46	47 min.
259	25-8-123567 L	7:59	9:05	66 min.
260	12-10-278977 L	7:59	8:08	9 min.
261	25-25-7865 L	7:59	8:48	49 min.
262	1-1-987453 L	7:59	-	-
263	1-11-1233 L	7:59	9:18	79 min
264	4 - 54 - 256428 L	8:00	8:30	30 min
265	8 - 58 - 256432 L	8:00	9:11	71 min.
266	14 - 21 - 9125661 L	8:01	8:10	9 min.
267	19 - 26 - 9125666 L	8:01	9:35	94 min.
268	22 - 29 - 9125669 L	8:02	8:09	7 min.
269	7 - 14 - 9125654 L	8:02	10:00	118 min.
270	12 - 19 - 9125659 L	8:03	-	-
271	10 - 26 - 3216898 L	8:03	8:30	27 min
272	24 - 40 - 3216912 L	8:03	10:24	141 min.
273	18 - 34 - 3216906 L	8:04	8:47	43 min.
274	21 - 71 - 256445 L	8:05	8:44	39 min
275	18 - 29 - 138993 L	8:05	8:12	7 min.
276	28 - 44 - 3216916 L	8:06	8:40	34 min.
277	20 - 50 - 5611343 L	8:06	8:19	13 min.
278	16 - 46 - 5611339 L	8:07	8:31	24 min
279	19 - 24 - 5984130 L	8:08	10:23	75 min,
280	41 - 72 - 1689455 L	8:08	8:14	6 min.
281	36 - 41 - 5984147 L	8:09	9:29	80 min.
282	39 - 55 - 2113686 L	8:10	9:15	65 min.
283	32 - 58 - 4768942 L	8:10	8:15	5 min.
284	4 - 30 - 4768914 L	8:11	10:12	121 min.
285	45 - 52 - 9125692 L	8:11	9:06	55 min.
286	31 - 81 - 256455 L	8:12	-	-
287	41 - 52 - 139016 L	8:13	8:35	22 min

No	Plate no	Arrival time	Dep. Time	duration
288	39 - 89 - 256463 L	8:14	9:01	47 min.
289	36 - 52 - 3216924 L	8:15	8:30	15 min.
290	21 - 37 - 2113668 L	8:16	9:34	78 min.
291	7 - 37 - 5611330 L	8:17	8:25	8 min.
292	14 - 25 - 138989 L	8:18	8:44	26 min.
293	49 - 69 - 1135539 L	8:18	9:27	69 min.
294	41 - 71 - 5611364 L	8:18	8:36	18 min.
295	49 - 54 - 5984160 L	8:19	-	-
296	36 - 56 - 1135526 L	8:19	9:14	55 min.
297	24 - 74 - 256448 L	8:19	8:26	7 min.
298	32 - 39 - 9125679 L	8:20	9:13	53 min.
299	14 - 45 - 1689428 L	8:20	9:05	45 min.
300	47 - 97 - 256471 L	8:20	10:11	101 min
301	35 - 46 - 139010 L	8:20	8:25	5 min.
302	36 - 47 - 139011 L	8:21	9:24	63 min.
303	16 - 36 - 1135506 L	8:21	10:32	131 min
304	31 - 61 - 5611354 L	8:21	9:02	41 min.
305	31 - 51 - 1135521 L	8:21	10:20	119 min
306	73 - 78 - 5984184 L	8:22	-	-
307	97 - 117 - 1135587 L	8:22	8:30	8 min.
308	90 - 97 - 9125737 L	8:23	9:58	95 min
309	82 - 98 - 2113729 L	8:23	8:58	35 min.
310	68 - 88 - 1135558 L	8:23	9:19	56 min.
311	62 - 73 - 139037 L	8:24	10:59	155 min
312	63 - 79 - 2113710 L	8:24	8:47	23 min.
313	73 - 99 - 4768983 L	8:24	10:37	133 min
314	69 - 76 - 9125716 L	8:24	9:41	77 min.
315	75 - 125 - 256499 L	8:24	9:18	54 min
316	81 - 111 - 5611404 L	8:24	11:21	173 min
317	79 - 95 - 3216967 L	8:24	9:36	72 min
318	78 - 109 - 1689492 L	8:24	8:31	7 min.

No	Plate no	Arrival time	Dep. Time	duration
319	43 - 73 - 5611366 L	8:25	9:18	53 min.
320	6 - 22 - 3216894 L	8:25	9:42	77 min.
321	5 - 21 - 2113652 L	8:25	10:39	134 min
322	22 - 42 - 1135512 L	8:25	9:50	85 min.
323	53 - 69 - 2113700 L	8:26	11:12	166 min
324	4 - 24 - 1135494 L	8:26	9:32	66 min.
325	66 - 77 - 139041 L	8:26	9:58	92 min.
326	59 - 85 - 4768969 L	8:27	11:03	156 min
327	3 - 19 - 2113650 L	8:27	9:15	48 min.
328	10 - 15 - 5984121 L	8:28	9:51	83 min.
329	18 - 68 - 256442 L	8:29	8:34	5 min.
330	76 - 102 - 4768986 L	8:29	11:24	175 min
331	76 - 106 - 5611399 L	8:29	8:54	25 min.
332	59 - 89 - 5611382 L	8:29	9:16	47 min.
333	8 - 28 - 1135498 L	8:29	10:13	104 min.
334	10 - 26 - 2113657 L	8:29	10:02	93 min.
335	6 - 13 - 9125653 L	8:29	8:57	28 min.
336	64 - 80 - 3216952 L	8:30	9:50	80min.
337	10 - 40 - 5611333 L	8:30	9:15	45 min.
338	85 - 92 - 9125732 L	8:30	8:36	6 min
339	70 - 100 - 5611393 L	8:30	10:11	101 min
340	4 - 20 - 3216892 L	8:30	10:26	116 min.
341	11 - 13 - 712452 L	8:31	9:28	57 min.
342	5 - 12 - 3125573 L	8:31	9:09	38 min.
343	25 - 53 - 7541156 L	8:31	10:21	110 min.
344	70 - 90 - 1135560 L	8:31	-	-
345	77 - 93 - 2113724 L	8:32	9:37	65 min
346	17 - 33 - 9133234 L	8:32	10:13	101 min.
347	96 - 112 - 2113743 L	8:32	8:53	21 min.
348	3 - 31 - 7541134 L	8:33	11:19	166 min
349	9 - 17 - 8159439 L	8:33	8:41	8 min.

No	Plate no	Arrival time	Dep. Time	duration
350	91 - 121 - 5611414 L	8:33	8:40	7 min.
351	92 - 142 - 256516 L	8:34	10:29	115 min.
352	22 - 40 - 1911252 L	8:34	-	-
353	14 - 32 - 1911244 L	8:34	9:20	46 min.
354	94 - 99 - 5984205 L	8: 34	9:12	38 min.
355	97 - 147 - 256521 L	8:35	9:28	53 min.
356	90 - 95 - 5984201 L	8:35	8:52	17 min.
357	85 - 90 - 5984196 L	8:35	-	-
358	10 - 17 - 3125578 L	8:35	8:44	9 min
359	16 - 18 - 712457 L	8:35	8:40	5 min.
360	88 - 108 - 1135578 L	8:36	10:30	114 min.
361	6 - 24 - 1911236 L	8:36	9:06	30 min.
362	16 - 28 - 8231582 L	8:36	10:05	89 min.
363	93 - 109 - 3216981 L	8:36	-	-
364	20 - 32 - 8231586 L	8:37	8:50	13 min.
365	93 - 104 - 139068 L	8:37	9:26	49 min.
366	15 - 47 - 4432602 L	8:39	10:12	93 min.
367	99 - 149 - 256523 L	8:39	10:08	89 min.
368	90 - 121 - 1689504 L	8:39	9:35	56 min.
369	10 - 28 - 1911240 L	8:39	8:58	19 min.
370	92 - 112 - 1135582 L	8:39	10:01	82 min.
371	20 - 22 - 712461 L	8:40	8:46	6 min
372	47 - 63 - 9133264 L	8:40	8:48	8 min.
373	47 - 55 - 8159477 L	8:40	9:20	40 min
374	35 - 53 - 1911265 L	8:41	10:20	99 min
375	56 - 84 - 7541187 L	8:41	9:24	43 min.
376	66 - 68 - 712507 L	8:42	8:51	9 min.
377	68 - 100 - 4432655 L	8:42	10:34	112 min
378	56 - 63 - 3125624 L	8:42	10:20	98 min
379	63 - 91 - 7541194 L	8:42	9:27	45 min
380	97 - 109 - 8231663 L	8:43	10:13	90 min

No	Plate no	Arrival time	Dep. Time	duration
381	88 - 106 - 1911318 L	8:43	8:49	6 min.
382	96 - 114 - 1911326 L	8:43	9:24	41 min.
383	91 - 93 - 712532 L	8:43	10:32	109 min
384	52 - 68 - 9133269 L	8:45	9:10	25 min.
385	28 - 30 - 712469 L	8:45	-	-
386	54 - 82 - 7541185 L	8:45	9:53	68 min.
387	27 - 34 - 3125595 L	8:45	8:53	8 min.
388	52 - 54 - 712493 L	8:45	9:16	31 min
389	25 - 43 - 1911255 L	8:45	9:19	34 min.
390	25 - 37 - 8231591 L	8:46	9:08	22 min
391	51 - 79 - 7541182 L	8:46	9:31	45 min.
392	54 - 61 - 3125622 L	8:46	10:01	75 min
393	19 - 26 - 3125587 L	8:46	10:45	119 min
394	40 - 72 - 4432627 L	8:46	-	-
395	71 - 79 - 8159501 L	8:47	10:11	84 min.
396	64 - 71 - 3125632 L	8:47	10:25	98 min
397	74 - 102 - 7541205 L	8:47	10:43	117 min
398	76 - 84 - 8159506 L	8:47	9:53	66 min.
399	52 - 70 - 1911282 L	8:48	9:31	43 min
400	53 - 61 - 8159483 L	8:48	9:27	39 min.
401	78 - 80 - 712519 L	8:48	10:30	102 min
402	58 - 90 - 4432645 L	8:48	9:13	25 min.
403	69 - 97 - 7541200 L	8:48	8:54	6 min.
404	66 - 78 - 8231632 L	8:49	10:25	96 min
405	60 - 88 - 7541191 L	8:49	10:06	77 min.
406	83 - 91 - 8159513 L	8:49	9:10	21 min.
407	83 - 90 - 3125651 L	8:49	9:29	40 min
408	82 - 100 - 1911312 L	8:49	9:22	33 min.
409	81 - 83 - 712522 L	8:49	9:18	29 min.
410	93 - 121 - 7541224 L	8:49	10:44	115 min

No	Plate no	Arrival time	Dep. Time	duration
411	91 - 98 - 3125659 L	8:49	11:02	133 min
412	89 - 105 - 9133306 L	8:50	9:08	18 min.
413	69 - 87 - 1911299 L	8:50	9:37	47 min
414	99 - 127 - 7541230 L	8:52	9:59	67 min.
415	85 - 97 - 8231651 L	8:50	10:21	91 min
416	99 - 125 - 4769009 L	8:50	-	-
417	7 - 19 - 8231573 L	8:50	9:32	42 min
418	95 - 102 - 9125742 L	8:50	10:29	99 min.
419	97 - 127 - 5611420 L	8:50	10:56	126 min
420	4 - 22 - 1911234 L	8:51	9:28	37 min.
421	13 - 21 - 8159443 L	8:51	10:30	101 min.
422	21 - 49 - 7541152 L	8:51	10:29	88 min.
423	6 - 22 - 9133223 L	8:51	8:56	5 min
424	4 - 36 - 4432591 L	8:51	9:05	14 min.
425	61 - 79 - 1911291 L	8:51	10:04	73 min
426	6 - 8 - 712447 L	8:51	10:42	111 min.
427	17 - 45 - 7541148 L	8:52	9:41	49 min.
428	23 - 30 - 3125591 L	8:52	9:57	65 min.
429	92 - 103 - 139067 L	8:52	-	-
430	82 - 108 - 4768992 L	8:52	8:59	7 min.
431	81 - 92 - 139056 L	8:52	8:58	6 min.
432	96 - 112 - 3216984 L	8:52	10:09	77 min.
433	89 - 105 - 2113736 L	8:52	9:25	33 min.
434	92 - 108 - 2113739 L	8:53	9:02	9 min.
435	86 - 102 - 2113733 L	8:53	11:02	129 min
436	53 - 103 - 256477 L	8:53	9:41	48 min.
437	50 - 81 - 1689464 L	8:53	-	-
438	58 - 78 - 1135548 L	8:53	9:35	42 min
439	44 - 75 - 1689458 L	8:53	11:15	142 min
440	55 - 85 - 5611378 L	8:53	9:40	47 min.
441	59 - 90 - 1689473 L	8:53	9:55	62 min.

No	Plate no	Arrival time	Dep. Time	duration
442	56 - 63 - 9125703 L	8:54	9:31	37 min.
443	75 - 91 - 9133292 L	8:54	10:29	95 min
444	65 - 95 - 5611388 L	8:54	11:26	152 min
445	49 - 65 - 3216937 L	8:54	9:30	36 min
446	59 - 70 - 139034 L	8:54	9:39	45 min.
447	39 - 65 - 4768949 L	8:54	10:45	111 min
448	61 - 68 - 9125708 L	8:54	10:16	82 min
449	41 - 61 - 1135531 L	8:55	9:47	52 min
450	58 - 74 - 2113705 L	8:55	9:39	44 min
451	60 - 71 - 139035 L	8:55	10:29	94 min
452	42 - 72 - 5611365 L	8:55	9:39	44 min.
453	62 - 78 - 2113709 L	8:55	9:17	22 min
454	96 - 98 - 712537 L	8:55	9:04	9 min.
455	42 - 73 - 1689456 L	8:55	9:19	24 min.
456	61 - 72 - 139036 L	8:56	10:41	113 min
457	43 - 74 - 1689457 L	8:56	9:38	42 min
458	53 - 79 - 4768963 L	8:56	9:45	49 min
459	1 - 51 - 256425 L	8:56	-	-
460	30 - 56 - 4768940 L	8:56	9:59	63 min.
461	5 - 21 - 3216893 L	8:56	10:37	101 min
462	28 - 39 - 139003 L	8:57	9:24	27 min.
463	1 - 27 - 4768911 L	8:57	10:29	92 min
464	30 - 60 - 5611353 L	8:57	9:52	55 min
465	10 - 17 - 9125657 L	8:57	9:40	43 min.
466	29 - 55 - 4768939 L	8:57	-	-
467	23 - 73 - 256447 L	8:57	10:12	75 min.
468	13 - 24 - 138988 L	8:58	9:26	28 min.
469	30 - 37 - 9125677 L	8:58	9:39	41 min.
470	1 - 17 - 3216889 L	8:58	9:21	23 min
471	29 - 49 - 1135519 L	8:58	10:06	68 min.
472	9 - 39 - 5611332 L	8:58	9:08	9 min

No	Plate no	Arrival time	Dep. Time	duration
473	30 - 46 - 2113677 L	8:58	10:08	70min.
474	5 - 36 - 1689419 L	8:59	9:34	35 min.
475	2 - 28 - 4768912 L	8:59	9:48	49 min.
476	40 - 90 - 256464 L	8:59	9:33	34 min.
477	58 - 69 - 139033 L	8:59	10:38	99 min
478	46 - 53 - 9125693 L	8:59	10:17	78 min.
479	37 - 53 - 3216925 L	8:59	9:37	38 min.
480	59 - 79 - 1135549 L	8:59	10:39	100 min
481	40 - 70 - 5611363 L	9:00	9:07	7 min.
482	66 - 71 - 5984177 L	9:00	9:59	59 min.
483	40 - 56 - 2113687 L	9:00	-	-
484	60 - 91 - 1689474 L	9:00	9:08	8 min.
485	66 - 92 - 4768976 L	9:00	9:57	57 min
486	76 - 126 - 256500 L	9:01	9:11	10 min
487	99 - 110 - 139074 L	9:01	10:09	68 min
488	73 - 80 - 9125720 L	9:01	9:46	45 min.
489	80 - 96 - 3216968 L	9:01	9:10	9 min.
490	89 - 109 - 1135579 L	9:01	10:05	64 min.
491	72 - 102 - 5611395 L	9:01	10:31	90 min.
492	99 - 104 - 5984210 L	9:02	9:52	50 min.
493	72 - 88 - 2113719 L	9:02	9:37	35 min.
494	89 - 120 - 1689503 L	9:02	9:46	44 min.
495	83 - 109 - 4768993 L	9:02	9:15	13 min.
496	1 - 3 - 712442 L	9:02	9:23	21 min.
497	27 - 35 - 8159457 L	9:02	10:17	75 min
498	8 - 20 - 8231574 L	9:03	9:10	7 min.
499	11 - 18 - 3125579 L	9:03	-	-
500	18 - 34 - 9133235 L	9:03	10:31	88 min
501	1 - 33 - 4432588 L	9:03	9:12	9 min.
502	27 - 45 - 1911257 L	9:03	9:48	45 min.
503	22 - 50 - 7541153 L	9:04	9:38	34 min.

No	Plate no	Arrival time	Dep. Time	duration
504	32 - 34 - 712473 L	9:04	10:19	75 min.
505	54 - 62 - 8159484 L	9:04	-	-
506	40 - 52 - 8231606 L	9:05	9:28	23 min.
507	55 - 62 - 3125623 L	9:05	9:20	15 min
508	48 - 64 - 9133265 L	9:05	10:37	92 min.
509	41 - 73 - 4432628 L	9:05	9:10	5 min.
510	51 - 69 - 1911281 L	9:06	9:45	39 min.
511	30 - 58 - 7541161 L	9:06	10:03	57 min.
512	67 - 69 - 712508 L	9:06	9:55	49 min.
513	94 - 122 - 7541225 L	9:06	10:48	102 min
514	83 - 101 - 1911313 L	9:06	10:42	96 min.
515	69 - 101 - 4432656 L	9:07	9:43	36 min.
516	88 - 104 - 9133305 L	9:07	9:24	17 min.
517	84 - 91 - 3125652 L	9:07	9:16	9 min.
518	84 - 96 - 8231650 L	9:07	10:13	66 min
519	82 - 90 - 8159512 L	9:07	10:59	112 min.
520	79 - 81 - 712520 L	9:08	11:37	149 min.
521	98 - 126 - 7541229 L	9:08	9:26	18 min
522	82 - 89 - 3125650 L	9:08	10:06	58 min.
523	99 - 107 - 8159529 L	9:08	9:57	49 min.
524	76 - 108 - 4432663 L	9:08	9:33	25 min.
525	99 - 106 - 3125667 L	9:08	10:14	66 min.
526	72 - 74 - 712513 L	9:09	-	-
527	83 - 95 - 8231649 L	9:09	9:15	6 min.
528	28 - 54 - 4768938 L	9:09	10:21	72 min
529	15 - 46 - 1689429 L	9:09	10:00	51 min.
530	35 - 40 - 5984146 L	9:09	9:49	40 min.
531	29 - 59 - 5611352 L	9:10	10:53	103 min
532	30 - 50 - 1135520 L	9:10	9:18	8 min.
533	19 - 35 - 3216907 L	9:10	10:49	99 min
534	25 - 41 - 3216913 L	9:10	10:17	67 min.

No	Plate no	Arrival time	Dep. Time	duration
535	19 - 30 - 138994 L	9:10	11:11	121 min
536	27 - 77 - 256451 L	9:11	9:53	42 min
537	36 - 43 - 9125683 L	9:11	10:48	97 min.
538	2 - 52 - 256426 L	9:11	9:17	6 min.
539	12 - 23 - 138987 L	9:11	-	-
540	23 - 30 - 9125670 L	9:11	9:56	45 min.
541	7 - 23 - 3216895 L	9:12	10:19	67 min
542	21 - 41 - 1135511 L	9:12	11:30	138 min.
543	1 - 31 - 5611324 L	9:12	10:23	71 min
544	9 - 14 - 5984120 L	9:12	9:54	42 min
545	6 - 37 - 1689420 L	9:12	11:21	129 min.
546	27 - 53 - 4768937 L	9:13	10:11	58 min.
547	3 - 53 - 256427 L	9:13	10:29	86 min
548	29 - 40 - 139004 L	9:13	10:32	89 min
549	1 - 8 - 9125648 L	9:13	10:32	79 min
550	2 - 18 - 3216890 L	9:13	10:44	91 min.
551	7 - 27 - 1135497 L	9:13	-	-
552	28 - 58 - 5611351 L	9:14	9:23	9 min.
553	1 - 6 - 5984112 L	9:14	9:54	40 min.
554	22 - 38 - 2113669 L	9:14	10:28	74 min
555	7 - 38 - 1689421 L	9:14	9:38	24 min.
556	26 - 52 - 4768936 L	9:14	10:10	56 min.
557	41 - 91 - 256465 L	9:15	10:01	46 min
558	33 - 44 - 139008 L	9:15	11:21	126 min.
559	62 - 69 - 9125709 L	9:15	10:52	97 min
560	38 - 54 - 3216926 L	9:15	9:50	35 min.
561	37 - 57 - 1135527 L	9:15	11:02	107 min.
562	60 - 90 - 5611383 L	9:15	10:03	48 min.
563	33 - 38 - 5984144 L	9:16	10:22	66 min
564	59 - 75 - 2113706 L	9:16	9:23	7 min.
565	45 - 76 - 1689459 L	9:16	9:53	37 min.

No	Plate no	Arrival time	Dep. Time	duration
566	74 - 100 - 4768984 L	9:16	-	-
567	54 - 104 - 256478 L	9:16	10:11	55 min.
568	80 - 91 - 139055 L	9:17	9:26	9 min,
569	81 - 88 - 9125728 L	9:17	10:06	49 min.
570	51 - 67 - 3216939 L	9:17	11:32	135 min.
571	57 - 77 - 1135547 L	9:17	9:40	23 min
572	77 - 107 - 5611400 L	9:17	10:54	97 min.
573	58 - 63 - 5984169 L	9:17	11:12	115 min.
574	81 - 97 - 2113728 L	9:18	-	-
575	58 - 89 - 1689472 L	9:18	10:06	48 min.
576	75 - 101 - 4768985 L	9:18	10:35	77 min
577	93 - 143 - 256517 L	9:18	10:15	57 min.
578	1 - 9 - 8159431 L	9:18	10:26	68 min.
579	96 - 103 - 9125743 L	9:18	9:23	5 min.
580	16 - 23 - 3125584 L	9:19	9:29	10 min.
581	93 - 113 - 1135583 L	9:19	10:05	46 min.
582	99 - 129 - 5611422 L	9:19	10:54	95 min.
583	97 - 102 - 5984208 L	9:19	10:51	92 min
584	18 - 46 - 7541149 L	9:19	11:28	129 min.
585	91 - 122 - 1689505 L	9:19	10:03	44 min
586	98 - 124 - 4769008 L	9:20	10:33	73 min
587	2 - 4 - 712443 L	9:20	9:59	39 min.
588	18 - 26 - 8159448 L	9:20	11:08	108 min
589	18 - 30 - 8231584 L	9:20	10:28	68 min
590	6 - 13 - 3125574 L	9:21	9:56	35 min.
591	92 - 122 - 5611415 L	9:21	10:42	81 min
592	13 - 31 - 1911243 L	9:21	10:38	77 min
593	13 - 41 - 7541144 L	9:21	10:09	48 min.
594	99 - 130 - 1689513 L	9:22	10:19	57 min.
595	90 - 116 - 4769000 L	9:22	10:36	74 min
596	23 - 25 - 712464 L	9:22	10:24	62 min

No	Plate no	Arrival time	Dep. Time	duration
597	48 - 56 - 8159478 L	9:22	10:47	87 min
598	26 - 38 - 8231592 L	9:22	10:13	51 min.
599	51 - 58 - 3125619 L	9:23	10:02	39 min.
600	34 - 84 - 256458 L	9:23	10:28	65 min
601	53 - 64 - 139028 L	9:23	10:30	67 min
602	29 - 36 - 9125676 L	9:23	10:49	86 min
603	57 - 73 - 3216945 L	9:23	9:37	14 min.
604	32 - 52 - 1135522 L	9:24	10:08	44 min.
605	44 - 74 - 5611367 L	9:24	12:13	169 min
606	56 - 82 - 4768966 L	9:24	10:52	88 min.
607	31 - 36 - 5984142 L	9:24	10:09	45 min
608	50 - 70 - 1135540 L	9:24	11:03	99 min.
609	50 - 66 - 3216938 L	9:24	12:21	177 min
610	31 - 38 - 9125678 L	9:25	10:00	35 min.
611	42 - 92 - 256466 L	9:25	10:14	49 min.
612	57 - 68 - 139032 L	9:25	10:40	75 min
613	69 - 89 - 1135559 L	9:25	10:44	79 min
614	68 - 98 - 5611391 L	9:25	9:38	13 min.
615	59 - 64 - 5984170 L	9:25	10:25	60 min.
616	79 - 90 - 139054 L	9:26	9:35	9 min.
617	54 - 80 - 4768964 L	9:26	-	-
618	84 - 100 - 2113731 L	9:26	11:05	99 min
619	80 - 130 - 256504 L	9:26	10:27	61 min
620	99 - 119 - 1135589 L	9:26	10:21	55 min.
621	1 - 29 - 7541132 L	9:27	10:00	33 min.
622	85 - 135 - 256509 L	9:27	9:39	12 min.
623	10 - 22 - 8231576 L	9:27	11:05	98 min
624	94 - 101 - 9125741 L	9:27	10:15	48 min.
625	90 - 106 - 2113737 L	9:27	9:36	9 min.
626	10 - 12 - 712451 L	9:28	-	-
627	92 - 123 - 1689506 L	9:28	10:43	75 min.

No	Plate no	Arrival time	Dep. Time	duration
628	2 - 30 - 7541133 L	9:28	10:34	66 min
629	67 - 117 - 256491 L	9:28	11:18	110 min
630	96 - 127 - 1689510 L	9:28	10:23	55 min..
631	93 - 100 - 9125740 L	9:29	10:15	46 min.
632	72 - 77 - 5984183 L	9:29	12:04	155 min.
633	68 - 99 - 1689482 L	9:29	9:46	17 min.
634	91 - 141 - 256515 L	9:29	-	-
635	77 - 97 - 1135567 L	9:29	9:51	22 min.
636	86 - 112 - 4768996 L	9:29	11:40	71 min
637	81 - 131 - 256505 L	9:30	11:45	85 min.
638	92 - 108 - 3216980 L	9:30	10:19	49 min.
639	67 - 72 - 5984178 L	9:30	9:55	25 min.
640	66 - 82 - 3216954 L	9:30	11:04	94 min.
641	1 - 17 - 2113648 L	9:30	10:46	86 min
642	5 - 55 - 256429 L	9:30	10:15	45 min.
643	15 - 22 - 9125662 L	9:31	10:05	34 min.
644	2 - 18 - 2113649 L	9:31	10:11	40 min
645	11 - 41 - 5611334 L	9:31	11:23	112 min.
646	17 - 67 - 256441 L	9:31	10:59	88 min
647	5 - 35 - 5611328 L	9:32	10:37	65 min
648	27 - 43 - 2113674 L	9:32	10:54	82 min
649	3 - 14 - 138978 L	9:32	10:12	40 min.
650	4 - 20 - 2113651 L	9:32	10:48	76 min
651	6 - 56 - 256430 L	9:32	9:58	26 min
652	1 - 21 - 1135491 L	9:33	10:28	55 min.
653	23 - 39 - 2113670 L	9:33	11:06	88 min.
654	1 - 12 - 138976 L	9:33	-	-
655	7 - 57 - 256431 L	9:33	11:16	103 min.
656	2 - 9 - 9125649 L	9:33	10:17	44 min.
657	20 - 40 - 1135510 L	9:34	10:07	33 min.
658	12 - 42 - 5611335 L	9:34	10:39	65 min

No	Plate no	Arrival time	Dep. Time	duration
659	24 - 40 - 2113671 L	9:34	12:29	175 min
660	26 - 76 - 256450 L	9:34	10:39	65 min.
661	6 - 22 - 2113653 L	9:34	10:08	24 min.
662	5 - 16 - 138980 L	9:34	11:29	115 min.
663	14 - 44 - 5611337 L	9:35	-	-
664	25 - 41 - 2113672 L	9:35	10:23	48 min.
665	2 - 22 - 1135492 L	9:35	-	-
666	16 - 23 - 9125663 L	9:35	10:52	77 min.
667	33 - 49 - 2113680 L	9:35	12:32	177 min.
668	9 - 59 - 256433 L	9:35	10:10	35 min.
669	20 - 36 - 2113667 L	9:35	9:58	23 min.
670	2 - 13 - 138977 L	9:36	12:17	161 min.
671	7 - 23 - 2113654 L	9:36	9:44	8 min.
672	25 - 75 - 256449 L	9:36	-	-
673	8 - 24 - 2113655 L	9:36	11:16	100 min.
674	10 - 60 - 256434 L	9:36	11:58	142 min.
675	4 - 15 - 138979 L	9:37	10:12	35 min.
676	23 - 43 - 1135513 L	9:37	11:44	127 min.
677	3 - 10 - 9125650 L	9:37	10:26	49 min.
678	13 - 43 - 5611336 L	9:37	9:46	9 min.
679	26 - 42 - 2113673 L	9:37	10:10	93 min
680	11 - 61 - 256435 L	9:37	10:23	46 min.
681	68 - 118 - 256492 L	9:38	10:14	36 min.
682	46 - 57 - 139021 L	9:38	11:06	88 min.
683	59 - 66 - 9125706 L	9:38	9:58	20 min
684	33 - 49 - 3216921 L	9:38	9:44	6 min.
685	64 - 84 - 1135554 L	9:39	11:44	125 min.
686	74 - 90 - 2113721 L	9:39	10:14	45 min,
687	22 - 53 - 1689436 L	9:39	12:22	163 min.
688	41 - 67 - 4768951 L	9:40	11:21	101 min.
689	74 - 124 - 256498 L	9:40	-	-

No	Plate no	Arrival time	Dep. Time	duration
690	30 - 41 - 139005 L	9:40	9:45	5 min.
691	31 - 57 - 4768941 L	9:40	10:44	64 min
692	71 - 91 - 1135561 L	9:41	10:29	48 min.
693	8 - 39 - 1689422 L	9:41	10:54	73 min
694	69 - 119 - 256493 L	9:41	10:04	23 min.
695	39 - 55 - 3216927 L	9:41	10:02	21 min.
696	2 - 7 - 5984113 L	9:41	10:45	64 min
697	33 - 59 - 4768943 L	9:41	12:35	174 min.
698	70 - 120 - 256494 L	9:42	10:57	75 min
699	48 - 59 - 139023 L	9:42	10:50	68 min
700	35 - 51 - 3216923 L	9:42	-	-
701	66 - 86 - 1135556 L	9:43	9:52	9 min
702	12 - 17 - 5984123 L	9:43	11:00	77 min.
703	43 - 69 - 4768953 L	9:43	10:30	47 min.
704	76 - 92 - 2113723 L	9:43	11:12	89 min.
705	24 - 55 - 1689438 L	9:44	12:15	151 min.
706	62 - 112 - 256486 L	9:44	10:17	33 min
707	30 - 61 - 1689444 L	9:44	11:17	93 min
708	25 - 51 - 4768935 L	9:44	-	-
709	77 - 127 - 256501 L	9:44	9:49	5 min.
710	3 - 8 - 5984114 L	9:45	10:42	57 min.
711	42 - 68 - 4768952 L	9:45	12:31	166 min.
712	55 - 105 - 256479 L	9:45	10:31	46 min.
713	60 - 80 - 1135550 L	9:45	10:15	30 min
714	4 - 9 - 5984115 L	9:46	12:05	139 min
715	73 - 89 - 2113720 L	9:46	9:55	9 min.
716	31 - 62 - 1689445 L	9:46	11:03	77 min
717	49 - 75 - 4768959 L	9:46	10:41	55 min.
718	71 - 121 - 256495 L	9:46	10:28	42 min
719	49 - 60 - 139024 L	9:47	9:53	6 min.
720	5 - 10 - 5984116 L	9:47	10:10	23 min

No	Plate no	Arrival time	Dep. Time	duration
721	67 - 87 - 1135557 L	9:47	11:26	99 min.
722	57 - 87 - 5611380 L	9:48	10:23	35 min.
723	29 - 60 - 1689443 L	9:48	9:56	8 min
724	44 - 70 - 4768954 L	9:48	11:21	93 min.
725	56 - 86 - 5611379 L	9:48	11:38	110 min.
726	47 - 58 - 139022 L	9:48	-	-
727	40 - 66 - 4768950 L	9:49	12:08	139 min.
728	72 - 122 - 256496 L	9:49	11:28	99 min.
729	89 - 96 - 9125736 L	9:49	10:11	22 min.
730	77 - 103 - 4768987 L	9:50	10:33	43 min.
731	93 - 123 - 5611416 L	9:50	11:19	89 min
732	3 - 5 - 712444 L	9:50	12:48	178 min
733	82 - 93 - 139057 L	9:50	-	-
734	96 - 126 - 5611419 L	9:50	11:11	81 min
735	4 - 32 - 7541135 L	9:51	10:46	55 min.
736	78 - 104 - 4768988 L	9:51	10:59	68 min
737	98 - 148 - 256522 L	9:51	10:36	45 min.
738	67 - 78 - 139042 L	9:51	10:42	51 min.
739	92 - 99 - 9125739 L	9:52	10:54	62 min
740	52 - 68 - 3216940 L	9:52	11:07	75 min
741	1 - 17 - 9133218 L	9:52	10:48	56 min.
742	78 - 108 - 5611401 L	9:52	11:15	83 min
743	53 - 58 - 5984164 L	9:53	-	-
744	97 - 113 - 2113744 L	9:53	10:00	7 min.
745	46 - 77 - 1689460 L	9:53	10:48	55 min.
746	72 - 98 - 4768982 L	9:53	10:25	32 min
747	24 - 26 - 712465 L	9:53	12:40	167 min.
748	17 - 25 - 8159447 L	9:53	10:01	8 min.
749	9 - 21 - 8231575 L	9:54	11:36	102 min.
750	1 - 8 - 3125569 L	9:54	11:04	70 min.
751	19 - 35 - 9133236 L	9:54	10:39	45 min.

No	Plate no	Arrival time	Dep. Time	duration
752	14 - 46 - 4432601 L	9:54	10:48	54 min.
753	68 - 73 - 5984179 L	9:55	10:01	6 min.
754	47 - 75 - 7541178 L	9:55	11:29	94 min.
755	79 - 110 - 1689493 L	9:55	11:32	97 min.
756	97 - 123 - 4769007 L	9:55	10:03	8 min.
757	40 - 42 - 712481 L	9:56	12:04	128 min.
758	46 - 54 - 8159476 L	9:56	11:41	105 min.
759	65 - 77 - 8231631 L	9:56	10:44	48 min.
760	28 - 35 - 3125596 L	9:56	10:54	58 min.
761	49 - 65 - 9133266 L	9:57	10:23	25 min.
762	59 - 91 - 4432646 L	9:57	10:08	11 min
763	1 - 19 - 1911231 L	9:57	10:37	40 min.
764	83 - 111 - 7541214 L	9:57	10:51	54 min.
765	92 - 94 - 712533 L	9:57	11:12	75 min
766	81 - 89 - 8159511 L	9 :57	10:42	45 min.
767	86 - 98 - 8231652 L	9:57	11:32	95 min.
768	53 - 60 - 3125621 L	9:58	10:04	6 min.
769	99 - 115 - 9133316 L	9:58	12:00	122 min.
770	70 - 102 - 4432657 L	9:58	10:54	56 min.
771	48 - 66 - 1911278 L	9:58	10:25	27 min.
772	97 - 125 - 7541228 L	9:58	11:57	119 min
773	98 - 106 - 8159528 L	9:58	12:48	170 min
774	99 - 111 - 8231665 L	9:59	11:22	83 min
775	92 - 99 - 3125660 L	9:59	10:21	22 min.
776	99 - 131 - 4432686 L	9:59	11:59	120 min
777	78 - 96 - 1911308 L	9:59	10:08	9 min
778	12 - 62 - 256436 L	10:00	11:07	67 min.
779	6 - 17 - 138981 L	10:00	10:42	42 min
780	18 - 25 - 9125665 L	10:00	-	-
781	24 - 44 - 1135514 L	10:01	11:29	88 min.
782	34 - 50 - 2113681 L	10:01	10:07	6 min.

No	Plate no	Arrival time	Dep. Time	duration
783	38 - 88 - 256462 L	10:01	10:41	40 min
784	44 - 51 - 9125691 L	10:02	10:40	38 min
785	3 - 19 - 3216891 L	10:02	11:17	75 min.
786	48 - 68 - 1135538 L	10:02	-	-
787	21 - 51 - 5611344 L	10:03	10:31	28 min
788	60 - 76 - 2113707 L	10:03	11:06	63 min.
789	1 - 32 - 1689415 L	10:03	10:12	9 min.
790	24 - 50 - 4768934 L	10:04	11:18	74 min
791	61 - 111 - 256485 L	10:04	-	-
792	63 - 74 - 139038 L	10:04	10:49	45 min
793	45 - 71 - 4768955 L	10:04	12:09	125 min.
794	32 - 63 - 1689446 L	10:04	10:34	30 min
795	75 - 91 - 2113722 L	10:04	10:26	22 min
796	6 - 11 - 5984117 L	10:04	10:32	88 min.
797	58 - 88 - 5611381 L	10:05	10:49	44 min
798	29 - 45 - 3216917 L	10:05	11:57	112 min
799	63 - 70 - 9125710 L	10:06	-	-
800	42 - 53 - 139017 L	10:07	11:12	65 min.
801	73 - 123 - 256497 L	10:07	11:48	41 min
802	71 - 101 - 5611394 L	10:07	11:42	95 min
803	47 - 63 - 3216935 L	10:08	11:03	55 min
804	78 - 98 - 1135568 L	10:08	10:51	43 min
805	66 - 116 - 256490 L	10:08	12:34	118 min
806	64 - 71 - 9125711 L	10:08	11:23	75 min.
807	56 - 67 - 139031 L	10:09	-	-
808	45 - 75 - 5611368 L	10:09	10:33	24 min
809	7 - 12 - 5984118 L	10:09	10:51	42 min
810	13 - 44 - 1689427 L	10:09	12:56	167 min.
811	55 - 81 - 4768965 L	10:10	10:17	7 min.
812	78 - 128 - 256502 L	10:10	11:02	52 min
813	91 - 102 - 139066 L	10:11	11:40	89 min.

No	Plate no	Arrival time	Dep. Time	duration
814	15 - 27 - 8231581 L	10:11	10:24	13 min.
815	68 - 84 - 3216956 L	10:11	10:53	42 min
816	7 - 23 - 9133224 L	10:11	10:48	37 min
817	2 - 34 - 4432589 L	10:12	11:45	93 min
818	50 - 55 - 5984161 L	10:12	10:29	17 min.
819	19 - 47 - 7541150 L	10:13	11:59	106 min
820	61 - 92 - 1689475 L	10:13	11:30	77 min.
821	96 - 122 - 4769006 L	10:13	11:01	48 min
822	4 - 6 - 712445 L	10:14	10:21	7 min.
823	2 - 10 - 8159432 L	10:14	10:40	26 min
824	11 - 23 - 8231577 L	10:14	12:14	120 min.
825	94 - 110 - 3216982 L	10:15	11:41	86 min.
826	81 - 97 - 3216969 L	10:15	11:11	56 min
827	2 - 18 - 9133219 L	10:15	11:54	99 min
828	16 - 48 - 4432603 L	10:16	11:44	88 min.
829	71 - 76 - 5984182 L	10:16	10:31	15 min.
830	31 - 59 - 7541162 L	10:17	10:25	8 min.
831	62 - 93 - 1689476 L	10:17	11:47	90 min
832	95 - 121 - 4769005 L	10:17	-	-
833	8 - 40 - 4432595 L	10:18	11:32	74 min.
834	77 - 93 - 3216965 L	10:18	-	-
835	41 - 43 - 712482 L	10:18	12:13	115 min
836	26 - 34 - 8159456 L	10:19	12:28	129 min.
837	27 - 39 - 8231593 L	10:19	12:11	52 min
838	12 - 19 - 3125580 L	10:19	10:55	36 min
839	51 - 67 - 9133268 L	10:20	11:01	41 min
840	17 - 49 - 4432604 L	10:20	10:38	18 min.
841	93 - 98 - 5984204 L	10:20	11:36	76 min.
842	46 - 74 - 7541177 L	10:21	10:48	27 min
843	98 - 129 - 1689512 L	10:21	10:26	5 min.
844	53 - 55 - 712494 L	10:21	11:28	67 min.

No	Plate no	Arrival time	Dep. Time	duration
845	5 - 13 - 8159435 L	10:22	11:10	48 min
846	47 - 59 - 8231613 L	10:22	11:03	41 min
847	97 - 113 - 3216985 L	10:22	12:03	101 min
848	37 - 69 - 4432624 L	10:23	11:13	50 min
849	80 - 85 - 5984191 L	10:23	11:56	93 min
850	39 - 41 - 712480 L	10:24	10:39	15 min.
851	19 - 27 - 8159449 L	10:24	10:54	30 min
852	28 - 40 - 8231594 L	10:25	11:35	70 min
853	2 - 9 - 3125570 L	10:25	10:34	9 min.
854	46 - 62 - 9133263 L	10:25	10:45	20 min
855	18 - 50 - 4432605 L	10:26	11:58	92 min
856	98 - 103 - 5984209 L	10:26	11:31	65 min.
857	61 - 89 - 7541192 L	10:26	11:10	44 min
858	97 - 128 - 1689511 L	10:27	11:08	41 min
859	56 - 58 - 712497 L	10:27	11:30	63 min.
860	6 - 14 - 8159436 L	10:27	12:18	111 min
861	39 - 51 - 8231605 L	10:28	11:23	57 min
862	20 - 27 - 3125588 L	10:28	10:33	5 min.
863	19 - 51 - 4432606 L	10:29	11:17	48 min
864	86 - 91 - 5984197 L	10:29	11:49	80 min.
865	73 - 75 - 712514 L	10:29	11:10	41 min
866	55 - 63 - 8159485 L	10:29	11:08	39 min
867	68 - 80 - 8231634 L	10:30	12:13	103 min
868	42 - 49 - 3125610 L	10:30	10:38	8 min.
869	73 - 89 - 9133290 L	10:30	11:19	49 min
870	64 - 96 - 4432651 L	10:31	12:57	146 min.
871	19 - 37 - 1911249 L	10:31	11:58	87 min.
872	96 - 124 - 7541227 L	10:32	10:39	7 min.
873	90 - 92 - 712531 L	10:32	12:04	102 min
874	41 - 49 - 8159471 L	10:33	10:55	22 min
875	82 - 94 - 8231648 L	10:33	11:17	44 min

No	Plate no	Arrival time	Dep. Time	duration
876	29 - 36 - 3125597 L	10:33	10:42	9 min
877	59 - 75 - 9133276 L	10:34	12:03	89 min.
878	50 - 82 - 4432637 L	10:34	12:13	99 min
879	5 - 23 - 1911235 L	10:34	11:29	55 min
880	99 - 101 - 712540 L	10:34	11:37	63 min.
881	77 - 85 - 8159507 L	10:35	-	-
882	90 - 102 - 8231656 L	10:35	12:27	112 min
883	95 - 111 - 9133312 L	10:35	11:09	34 min
884	86 - 118 - 4432673 L	10:36	12:50	134 min.
885	41 - 59 - 1911271 L	10:36	11:22	46 min
886	97 - 105 - 8159527 L	10:36	10:58	22 min
887	98 - 110 - 8231664 L	10:37	11:00	23 min
888	81 - 88 - 3125649 L	10:37	11:30	53 min
889	98 - 114 - 9133315 L	10:38	12:09	91 min
890	98 - 130 - 4432685 L	10:39	11:57	78 min.
891	67 - 85 - 1911297 L	10:39	11:19	40 min
892	74 - 82 - 8159504 L	10:40	11:23	43 min
893	61 - 68 - 3125629 L	10:40	10:49	9 min.
894	87 - 99 - 8231653 L	10:40	10:57	17 min.
895	83 - 115 - 4432670 L	10:40	11:48	68 min.
896	92 - 108 - 9133309 L	10:41	-	-
897	97 - 115 - 1911327 L	10:41	11:19	38 min
898	98 - 105 - 3125666 L	10:41	10:47	6 min.
899	96 - 104 - 8159526 L	10:42	12:13	91 min
900	95 - 107 - 8231661 L	10:42	11:38	56 min
901	98 - 100 - 712539 L	10:42	11:37	45 min
902	99 - 117 - 1911329 L	10:43	10:55	12 min.
903	9 - 25 - 2113656 L	10:43	10:51	8 min.
904	13 - 63 - 256437 L	10:43	11:06	23 min
905	4 - 11 - 9125651 L	10:44	11:53	69 min.
906	19 - 39 - 1135509 L	10:44	12:59	135 min.

No	Plate no	Arrival time	Dep. Time	duration
907	28 - 44 - 2113675 L	10:45	10:50	5 min.
908	28 - 78 - 256452 L	10:45	11:31	46 min
909	31 - 42 - 139006 L	10:46	12:20	94 min
910	24 - 31 - 9125671 L	10:46	10:52	6 min.
911	8 - 24 - 3216896 L	10:46	11:49	63 min.
912	47 - 67 - 1135537 L	10:47	11:03	16 min.
913	32 - 62 - 5611355 L	10:48	-	-
914	57 - 73 - 2113704 L	10:48	11:59	71 min
915	2 - 33 - 1689416 L	10:48	11:27	39 min
916	3 - 29 - 4768913 L	10:49	12:21	92 min
917	65 - 115 - 256489 L	10:49	10:56	7 min.
918	55 - 66 - 139030 L	10:50	12:42	112 min
919	57 - 64 - 9125704 L	10:50	11:03	13 min.
920	40 - 56 - 3216928 L	10:50	11:22	32 min
921	61 - 81 - 1135551 L	10:50	11:30	40 min
922	61 - 91 - 5611384 L	10:51	11:49	58 min
923	78 - 94 - 2113725 L	10:51	12:18	78 min.
924	16 - 47 - 1689430 L	10:51	10:59	8 min.
925	34 - 60 - 4768944 L	10:51	-	-
926	94 - 144 - 256518 L	10:52	11:09	17 min,
927	83 - 94 - 139058 L	10:52	12:06	74 min.
928	1 - 13 - 8231567 L	10:52	11:18	26 min
929	78 - 94 - 3216966 L	10:52	12:45	113 min
930	94 - 114 - 1135584 L	10:53	12:13	80 min.
931	98 - 128 - 5611421 L	10:53	11:26	33 min
932	37 - 42 - 5984148 L	10:53	12:45	112 min
933	16 - 44 - 7541147 L	10:53	11:34	41 min
934	63 - 94 - 1689477 L	10:54	11:03	9 min.
935	79 - 105 - 4768989 L	10:54	11:42	48 min
936	5 - 7 - 712446 L	10:54	11:25	31 min
937	5 - 21 - 9133222 L	10:54	-	-

No	Plate no	Arrival time	Dep. Time	duration
938	64 - 95 - 1689478 L	10:54	12:25	91 min
939	38 - 40 - 712479 L	10:55	11:58	63 min.
940	39 - 47 - 8159469 L	10:55	11:37	42 min
941	41 - 53 - 8231607 L	10:55	11:02	7 min.
942	21 - 28 - 3125589 L	10:56	-	-
943	39 - 55 - 9133256 L	10:56	13:14	138 min.
944	39 - 71 - 4432626 L	10:56	11:47	51 min
945	87 - 92 - 5984198 L	10:56	11:25	29 min
946	64 - 92 - 7541195 L	10:56	11:01	5 min.
947	95 - 126 - 1689509 L	10:57	11:41	44 min
948	37 - 39 - 712478 L	10:57	12:27	90 min
949	50 - 66 - 9133267 L	10:57	12:18	81 min.
950	32 - 40 - 8159462 L	10:57	11:14	17 min.
951	3 - 10 - 3125571 L	10:57	-	-
952	30 - 42 - 8231596 L	10:57	11:05	8 min.
953	44 - 76 - 4432631 L	10:58	11:42	44 min
954	80 - 82 - 712521 L	10:58	12:10	72 min.
955	63 - 71 - 8159493 L	10:58	11:12	14 min.
956	95 - 123 - 7541226 L	10:58	11:31	33 min
957	45 - 63 - 1911275 L	10:58	11:59	61 min.
958	87 - 119 - 4432674 L	10:58	11:13	15 min.
959	87 - 103 - 9133304 L	10:59	11:20	21 min
960	57 - 64 - 3125625 L	10:59	11:05	6 min.
961	81 - 93 - 8231647 L	10:59	11:42	43min
962	70 - 78 - 8159500 L	10:59	11:09	10 min.
963	93 - 95 - 712534 L	11:00	-	-
964	60 - 78 - 1911290 L	11:00	12:01	61 min.
965	97 - 129 - 4432684 L	11:01	11:08	7 min,
966	97 - 113 - 9133314 L	11:01	11:52	51 min
967	80 - 87 - 3125648 L	11:01	11:22	21 min
968	96 - 108 - 8231662 L	11:02	12:35	93 min

No	Plate no	Arrival time	Dep. Time	duration
969	97 - 99 - 712538 L	11:02	13:24	142 min.
970	98 - 116 - 1911328 L	11:02	11:24	22 min
971	14 - 64 - 256438 L	11:03	11:59	56 min
972	5 - 12 - 9125652 L	11:04	12:10	66 min.
973	3 - 23 - 1135493 L	11:04	11:51	47 min
974	11 - 27 - 2113658 L	11:05	11:38	33 min
975	2 - 32 - 5611325 L	11:06	11:11	5 min.
976	15 - 65 - 256439 L	11:06	12:49	103 min
977	8 - 15 - 9125655 L	11:06	12:03	57 min
978	18 - 38 - 1135508 L	11:06	-	-
979	15 - 45 - 5611338 L	11:07	12:24	77 min.
980	29 - 45 - 2113676 L	11:07	11:31	24 min
981	48 - 98 - 256472 L	11:08	11:17	9 min.
982	26 - 37 - 139001 L	11:08	13:06	118 min
983	39 - 46 - 9125686 L	11:09	13:27	138 min
984	13 - 29 - 3216901 L	11:09	11:33	24 min
985	44 - 64 - 1135534 L	11:10	11:17	7 min.
986	35 - 65 - 5611358 L	11:10	12:35	85 min.
987	54 - 70 - 2113701 L	11:11	11:47	36 min
988	21 - 47 - 4768931 L	11:11	11:30	19 mi
989	64 - 114 - 256488 L	11:12	11:55	43 min
990	65 - 72 - 9125712 L	11:12	11:54	42 min
991	54 - 65 - 139029 L	11:12	11:19	7 min.
992	30 - 46 - 3216918 L	11:12	12:41	89 min.
993	76 - 96 - 1135566 L	11:13	12:04	51 min
994	46 - 76 - 5611369 L	11:13	13:34	141 min.
995	27 - 32 - 5984138 L	11:14	13:06	112 min
996	64 - 80 - 2113711 L	11:15	11:48	33 min
997	39 - 70 - 1689453 L	11:15	13:29	134 min
998	35 - 61 - 4768945 L	11:16	12:22	66 min.
999	88 - 138 - 256512 L	11:17	12:02	45 min

No	Plate no	Arrival time	Dep. Time	duration
1000	89 - 100 - 139064 L	11:18	13:59	161 min
1001	91 - 98 - 9125738 L	11:19	11:35	16 min
1002	76 - 92 - 3216964 L	11:19	11:41	22 min
1003	3 - 19 - 9133220 L	11:19	12:12	53 min
1004	95 - 125 - 5611418 L	11:19	11:48	29 min
1005	38 - 43 - 5984149 L	11:20	12:58	98 min
1006	15 - 43 - 7541146 L	11:20	12:00	40 min
1007	40 - 71 - 1689454 L	11:21	12:38	77 min.
1008	84 - 110 - 4768994 L	11:21	13:41	140 min
1009	25 - 27 - 712466 L	11:22	13:23	121 min
1010	3 - 11 - 8159433 L	11:22	-	-
1011	29 - 41 - 8231595 L	11:23	11:50	27 min
1012	95 - 111 - 3216983 L	11:23	12:00	37 min
1013	40 - 56 - 9133257 L	11:23	12:20	57 min
1014	9 - 41 - 4432596 L	11:24	11:32	8 min.
1015	88 - 93 - 5984199 L	11:24	11:42	18 min
1016	32 - 60 - 7541163 L	11:25	12:49	84 min.
1017	48 - 76 - 7541179 L	11:26	13:11	105 min
1018	42 - 44 - 712483 L	11:27	11:33	6 min.
1019	4 - 12 - 8159434 L	11:27	12:59	92 min
1020	31 - 43 - 8231597 L	11:28	12:03	35 min
1021	91 - 107 - 3216979 L	11:29	11:43	14 min
1022	20 - 36 - 9133237 L	11:30	11:57	27 min
1023	10 - 42 - 4432597 L	11:30	12:37	67 min.
1024	84 - 89 - 5984195 L	11:31	12:12	41 min
1025	33 - 61 - 7541164 L	11:31	12:24	53 min
1026	94 - 125 - 1689508 L	11:31	13:10	99 min
1027	93 - 119 - 4769003 L	11:32	12:03	31 min
1028	65 - 67 - 712506 L	11:32	12:47	75 min.
1029	43 - 51 - 8159473 L	11:33	12:15	42 min
1030	56 - 68 - 8231622 L	11:33	11:52	19 min

No	Plate no	Arrival time	Dep. Time	duration
1031	30 - 37 - 3125598 L	11:33	11:52	49 min
1032	61 - 77 - 9133278 L	11:34	13:37	123 min
1033	52 - 84 - 4432639 L	11:34	-	-
1034	7 - 25 - 1911237 L	11:35	12:31	56 min
1035	71 - 99 - 7541202 L	11:36	11:45	9 min.
1036	82 - 84 - 712523 L	11:37	12:55	78 min.
1037	60 - 68 - 8159490 L	11:37	13:21	104 min
1038	80 - 92 - 8231646 L	11:38	14:02	144 min
1039	31 - 38 - 3125599 L	11:39	11:50	11 min
1040	91 - 107 - 9133308 L	11:39	12:42	63 min.
1041	82 - 114 - 4432669 L	11:40	-	-
1042	8 - 26 - 1911238 L	11:41	11:48	7 min.
1043	92 - 120 - 7541223 L	11:41	12:37	56 min
1044	75 - 83 - 8159505 L	11:41	12:09	28 min
1045	94 - 106 - 8231660 L	11:42	13:25	103 min
1046	85 - 92 - 3125653 L	11:42	13:10	88 min.
1047	96 - 112 - 9133313 L	11:43	13:44	121 min
1048	96 - 128 - 4432683 L	11:43	13:15	92 min.
1049	42 - 60 - 1911272 L	11:44	12:11	27 min
1050	97 - 104 - 3125665 L	11:45	11:59	14 min
1051	95 - 113 - 1911325 L	11:45	12:19	35 min
1052	16 - 66 - 256440 L	11:46	13:35	109 min
1053	5 - 25 - 1135495 L	11:46	12:38	52 min
1054	32 - 48 - 2113679 L	11:46	11:55	9 min.
1055	3 - 33 - 5611326 L	11:47	14:08	141 min
1056	29 - 79 - 256453 L	11:48	-	-
1057	7 - 18 - 138982 L	11:48	14:00	132 min
1058	20 - 27 - 9125667 L	11:48	13:11	83 min.
1059	25 - 45 - 1135515 L	11:49	13:30	101 min
1060	35 - 51 - 2113682 L	11:49	12:48	59 min
1061	5 - 31 - 4768915 L	11:50	11:56	6 min.

No	Plate no	Arrival time	Dep. Time	duration
1062	17 - 24 - 9125664 L	11:50	12:42	52 min
1063	31 - 47 - 2113678 L	11:50	14:08	138 min
1064	32 - 43 - 139007 L	11:51	13:04	73 min.
1065	20 - 36 - 3216908 L	11:51	12:30	39 min
1066	51 - 71 - 1135541 L	11:52	12:00	8 min.
1067	61 - 77 - 2113708 L	11:52	13:24	92 min
1068	9 - 40 - 1689423 L	11:53	12:18	25 min
1069	50 - 61 - 139025 L	11:53	12:02	9 min.
1070	14 - 19 - 5984125 L	11:53	13:22	89 min.
1071	26 - 57 - 1689440 L	11:54	12:45	51 min
1072	86 - 136 - 256510 L	11:54	12:05	11 min
1073	64 - 75 - 139039 L	11:55	12:00	5 min.
1074	77 - 84 - 9125724 L	11:55	12:13	18 min
1075	82 - 102 - 1135572 L	11:55	12:57	62 min.
1076	73 - 103 - 5611396 L	11:55	14:06	131 min
1077	28 - 33 - 5984139 L	11:56	12:01	5 min.
1078	7 - 9 - 712448 L	11:57	13:02	65 min.
1079	87 - 98 - 139062 L	11:57	-	-
1080	74 - 90 - 3216962 L	11:57	12:19	22 min
1081	51 - 56 - 5984162 L	11:58	12:29	31 min
1082	21 - 23 - 712462 L	11:58	13:10	72 min,
1083	12 - 24 - 8231578 L	11:58	12:05	7 min.
1084	88 - 104 - 3216976 L	11:58	12:53	55 min
1085	65 - 70 - 5984176 L	11:59	13:00	61 min.
1086	27 - 55 - 7541158 L	12:00	14:01	121 min
1087	77 - 108 - 1689491 L	12:00	13:52	112 min
1088	16 - 24 - 8159446 L	12:01	13:29	88 min.
1089	34 - 50 - 9133251 L	12:01	12:10	9 min
1090	25 - 57 - 4432612 L	12:02	12:13	11 min
1091	82 - 87 - 5984193 L	12:03	-	-
1092	44 - 72 - 7541175 L	12:03	12:45	42 min

No	Plate no	Arrival time	Dep. Time	duration
1093	77 - 79 - 712518 L	12:04	13:19	75 min.
1094	31 - 39 - 8159461 L	12:04	12:31	27 min
1095	67 - 79 - 8231633 L	12:05	12:12	7 min.
1096	22 - 29 - 3125590 L	12:05	12:38	33 min
1097	74 - 90 - 9133291 L	12:06	14:23	137 min
1098	9 - 27 - 1911239 L	12:07	12:58	51 min
1099	84 - 112 - 7541215 L	12:08	13:08	60 min.
1100	95 - 97 - 712536 L	12:08	13:47	99 min
1101	73 - 81 - 8159503 L	12:09	13:07	58 min
1102	60 - 67 - 3125628 L	12:09	13:55	106 min
1103	37 - 55 - 1911267 L	12:10	12:46	36 min
1104	95 - 103 - 8159525 L	12:11	12:19	8 min
1105	59 - 77 - 1911289 L	12:11	12:36	25 min
1106	89 - 91 - 712530 L	12:11	14:30	139 min
1107	94 - 102 - 8159524 L	12:11	13:21	70 min.
1108	93 - 105 - 8231659 L	12:12	14:13	121 min
1109	96 - 103 - 3125664 L	12:12	12:45	33 min
1110	78 - 106 - 7541209 L	12:13	13:52	99 min
1111	20 - 38 - 1911250 L	12:13	12:34	21 min
1112	12 - 28 - 2113659 L	12:14	13:43	89 min.
1113	6 - 26 - 1135496 L	12:15	14:20	125 min
1114	4 - 34 - 5611327 L	12:17	13:10	53 min
1115	9 - 16 - 9125656 L	12:17	12:54	37 min
1116	19 - 69 - 256443 L	12:18	14:33	135 min.
1117	43 - 93 - 256467 L	12:19	14:01	102 min
1118	22 - 33 - 138997 L	12:21	13:56	95 min
1119	49 - 99 - 256473 L	12:21	13:10	49 min
1120	27 - 38 - 139002 L	12:22	12:44	22 min
1121	40 - 47 - 9125687 L	12:22	13:39	87 min.
1122	14 - 30 - 3216902 L	12:24	14:52	148 min
1123	45 - 65 - 1135535 L	12:25	12:43	18 min

No	Plate no	Arrival time	Dep. Time	duration
1124	36 - 66 - 5611359 L	12:26	12:34	8 min.
1125	55 - 71 - 2113702 L	12:27	13:08	41 min
1126	3 - 34 - 1689417 L	12:27	-	-
1127	22 - 48 - 4768932 L	12:28	13:23	55 min
1128	50 - 100 - 256474 L	12:29	12:56	27 min
1129	41 - 48 - 9125688 L	12:29	13:13	44 min
1130	15 - 31 - 3216903 L	12:30	13:37	67 min.
1131	46 - 66 - 1135536 L	12:31	12:36	5 min.
1132	37 - 67 - 5611360 L	12:32	14:56	144 min
1133	56 - 72 - 2113703 L	12:33	13:12	39 min
1134	4 - 35 - 1689418 L	12:33	14:24	111 min
1135	23 - 49 - 4768933 L	12:34	12:48	14 min
1136	43 - 54 - 139018 L	12:34	13:23	53 min.
1137	52 - 82 - 5611375 L	12:35	12:51	16 min
1138	71 - 87 - 2113718 L	12:36	12:43	7 min.
1139	19 - 50 - 1689433 L	12:38	-	-
1140	38 - 64 - 4768948 L	12:38	13:56	78 min.
1141	79 - 129 - 256503 L	12:39	13:13	34 min
1142	70 - 77 - 9125717 L	12:40	14:32	112 min
1143	44 - 60 - 3216932 L	12:40	13:43	63 min.
1144	75 - 95 - 1135565 L	12:41	13:33	52 min
1145	66 - 96 - 5611389 L	12:42	12:50	8 min.
1146	21 - 26 - 5984132 L	12:42	13:09	27 min
1147	85 - 101 - 2113732 L	12:43	14:42	119 min
1148	33 - 64 - 1689447 L	12:44	13:33	49 min
1149	52 - 78 - 4768962 L	12:44	14:59	135 min
1150	95 - 145 - 256519 L	12:45	12:54	9 min.
1151	73 - 84 - 139048 L	12:46	13:14	28 min
1152	86 - 93 - 9125733 L	12:47	13:28	41 min
1153	60 - 76 - 3216948 L	12:48	13:27	39 min
1154	91 - 111 - 1135581 L	12:48	13:49	61 min.

No	Plate no	Arrival time	Dep. Time	duration
1155	82 - 112 - 5611405 L	12:49	14:27	98 min
1156	49 - 80 - 1689463 L	12:49	-	-
1157	68 - 94 - 4768978 L	12:50	15:40	170 min
1158	96 - 146 - 256520 L	12:50	13:06	16 min
1159	74 - 85 - 139049 L	12:51	14:51	120 min.
1160	87 - 94 - 9125734 L	12:52	13:21	29 min
1161	61 - 77 - 3216949 L	12:52	13:44	52 min
1162	83 - 113 - 5611406 L	12:52	14:43	111 min
1163	69 - 95 - 4768979 L	12:52	13:00	8 min.
1164	13 - 29 - 2113660 L	12:53	13:26	33 min
1165	9 - 29 - 1135499 L	12:53	14:32	99 min
1166	20 - 70 - 256444 L	12:54	13:42	48 min
1167	11 - 18 - 9125658 L	12:54	13:19	25 min
1168	14 - 30 - 2113661 L	12:55	14:12	77 min
1169	94 - 112 - 1911324 L	12:55	15:00	125 min
1170	22 - 72 - 256446 L	12:55	13:00	5 min.
1171	13 - 20 - 9125660 L	12:56	14:59	123 min
1172	8 - 38 - 5611331 L	12:56	14:34	98 min
1173	15 - 31 - 2113662 L	12:56	13:43	47 min
1174	46 - 96 - 256470 L	12:57	15:27	150 min
1175	24 - 35 - 138999 L	12:57	13:18	21 min
1176	37 - 44 - 9125684 L	12:58	13:05	7 min.
1177	11 - 27 - 3216899 L	12:58	13:20	22 min
1178	42 - 62 - 1135532 L	12:58	13:51	53 min
1179	33 - 63 - 5611356 L	12:59	13:25	26 min
1180	52 - 68 - 2113699 L	12:59	13:15	16 min
1181	19 - 45 - 4768929 L	12:59	13:08	9 min.
1182	25 - 36 - 139000 L	13:00	13:33	33 min
1183	38 - 45 - 9125685 L	13:00	14:05	65 min.
1184	12 - 28 - 3216900 L	13:00	13:45	45 min
1185	43 - 63 - 1135533 L	13:00	13:16	16 min

No	Plate no	Arrival time	Dep. Time	duration
1186	34 - 64 - 5611357 L	13:00	13:43	43 min
1187	16 - 32 - 2113663 L	13:01	13:08	7 min.
1188	82 - 132 - 256506 L	13:01	14:18	77 min.
1189	8 - 19 - 138983 L	13:01	13:33	32 min
1190	55 - 62 - 9125702 L	13:01	-	-
1191	9 - 25 - 3216897 L	13:01	13:06	5 min.
1192	62 - 82 - 1135552 L	13:01	-	-
1193	27 - 57 - 5611350 L	13:01	13:33	32 min
1194	17 - 33 - 2113664 L	13:02	15:02	60 min.
1195	83 - 133 - 256507 L	13:02	13:54	52 min
1196	74 - 81 - 9125721 L	13:02	13:29	27 min
1197	48 - 64 - 3216936 L	13:02	14:41	99 min
1198	79 - 99 - 1135569 L	13:02	14:30	88 min.
1199	25 - 30 - 5984136 L	13:03	13:43	40 min
1200	37 - 68 - 1689451 L	13:03	13:12	9 min.
1201	90 - 140 - 256514 L	13:03	13:47	44 min
1202	68 - 79 - 139043 L	13:03	15:15	132 min
1203	55 - 71 - 3216943 L	13:03	14:25	82 min.
1204	86 - 106 - 1135576 L	13:04	13:14	10 min
1205	32 - 37 - 5984143 L	13:04	13:47	43 min
1206	63 - 89 - 4768973 L	13:04	14:08	64 min.
1207	78 - 89 - 139053 L	13:04	14:36	92 min
1208	65 - 81 - 3216953 L	13:04	13:13	9 min.
1209	96 - 116 - 1135586 L	13:05	13:27	22 min
1210	87 - 117 - 5611410 L	13:05	14:20	75 min.
1211	42 - 47 - 5984153 L	13:05	15:03	118 min
1212	54 - 85 - 1689468 L	13:05	13:47	42 min
1213	12 - 14 - 712453 L	13:05	13:56	51 min
1214	3 - 15 - 8231569 L	13:05	15:16	131 min.
1215	8 - 24 - 9133225 L	13:06	-	-
1216	56 - 61 - 5984167 L	13:06	13:12	6 min.

No	Plate no	Arrival time	Dep. Time	duration
1217	87 - 113 - 4768997 L	13:06	13:49	43 min
1218	31 - 33 - 712472 L	13:06	14:39	93 min
1219	22 - 34 - 8231588 L	13:06	15:06	120 min
1220	98 - 114 - 3216986 L	13:06	15:00	114 min
1221	27 - 43 - 9133244 L	13:06	13:15	9 min.
1222	75 - 80 - 5984186 L	13:07	13:41	34 min
1223	37 - 65 - 7541168 L	13:07	13:58	51 min
1224	87 - 118 - 1689501 L	13:07	14:47	100 min
1225	85 - 93 - 8159515 L	13:07	14:25	78 min.
1226	72 - 79 - 3125640 L	13:07	13:51	44 min
1227	93 - 111 - 1911323 L	13:07	13:30	23 min
1228	76 - 78 - 712517 L	13:08	15:15	127 min
1229	41 - 48 - 3125609 L	13:08	13:57	49 min
1230	72 - 88 - 9133289 L	13:08	14:52	104 min
1231	63 - 95 - 4432650 L	13:08	13:50	42 min
1232	92 - 110 - 1911322 L	13:08	14:13	65 min
1233	43 - 45 - 712484 L	13:09	13:35	26 min
1234	21 - 29 - 8159451 L	13:09	13:58	49 min
1235	34 - 46 - 8231600 L	13:09	13:50	41 min
1236	16 - 32 - 3216904 L	13:09	13:39	30 min
1237	10 - 30 - 1135500 L	13:09	14:06	57 min
1238	38 - 68 - 5611361 L	13:10	14:15	65 min.
1239	8 - 13 - 5984119 L	13:10	-	-
1240	18 - 34 - 2113665 L	13:10	13:34	24 min
1241	10 - 41 - 1689424 L	13:10	-	-
1242	6 - 32 - 4768916 L	13:10	13:18	8 min.
1243	26 - 28 - 712467 L	13:10	13:26	16 min
1244	17 - 29 - 8231583 L	13:11	15:10	119 min
1245	23 - 39 - 9133240 L	13:11	13:59	48 min
1246	83 - 114 - 1689497 L	13:11	14:39	88 min.
1247	75 - 77 - 712516 L	13:11	14:04	53 min

No	Plate no	Arrival time	Dep. Time	duration
1248	40 - 47 - 3125608 L	13:12	13:39	27 min
1249	71 - 87 - 9133288 L	13:12	13:59	47 min
1250	62 - 94 - 4432649 L	13:13	13:46	33 min
1251	17 - 35 - 1911247 L	13:13	14:45	92 min
1252	81 - 109 - 7541212 L	13:13	15:03	110 min
1253	83 - 85 - 712524 L	13:14	13:49	35 min
1254	9 - 20 - 138984 L	13:15	14:10	55 min
1255	35 - 42 - 9125682 L	13:16	13:59	43 min
1256	11 - 31 - 1135501 L	13:16	14:20	64 min
1257	6 - 36 - 5611329 L	13:16	-	-
1258	19 - 35 - 2113666 L	13:17	14:13	55 min
1259	30 - 80 - 256454 L	13:17	14:01	44 min
1260	10 - 21 - 138985 L	13:17	15:22	125 min
1261	21 - 28 - 9125668 L	13:18	13:52	34 min
1262	12 - 32 - 1135502 L	13:18	14:36	78 min.
1263	17 - 47 - 5611340 L	13:18	13:26	8 min.
1264	36 - 52 - 2113683 L	13:19	-	-
1265	11 - 16 - 5984122 L	13:19	14:08	49 min
1266	11 - 42 - 1689425 L	13:19	14:53	94 min
1267	7 - 33 - 4768917 L	13:20	15:32	132 min
1268	34 - 42 - 8159464 L	13:20	14:57	97 min
1269	43 - 75 - 4432630 L	13:21	13:41	20 min
1270	62 - 90 - 7541193 L	13:21	14:50	89 min.
1271	57 - 59 - 712498 L	13:22	14:03	41 min
1272	35 - 43 - 8159465 L	13:22	13:34	12 min
1273	48 - 60 - 8231614 L	13:22	14:21	59 min
1274	44 - 52 - 8159474 L	13:22	15:00	98 min
1275	57 - 69 - 8231623 L	13:22	15:04	102 min
1276	62 - 78 - 9133279 L	13:23	14:24	61 min.
1277	53 - 85 - 4432640 L	13:23	13:55	32 min
1278	72 - 100 - 7541203 L	13:23	13:28	5 min.

No	Plate no	Arrival time	Dep. Time	duration
1279	56 - 64 - 8159486 L	13:23	13:45	22 min
1280	69 - 81 - 8231635 L	13:23	13:33	10 min
1281	43 - 50 - 3125611 L	13:23	14:09	46 min
1282	65 - 97 - 4432652 L	13:24	-	-
1283	88 - 90 - 712529 L	13:24	13:30	6 min.
1284	66 - 74 - 8159496 L	13:24	14:08	44 min
1285	79 - 91 - 8231645 L	13:24	14:30	66 min.
1286	84 - 100 - 9133301 L	13:24	14:58	94 min
1287	75 - 107 - 4432662 L	13:24	13:31	7 min.
1288	30 - 48 - 1911260 L	13:24	15:03	109 min
1289	94 - 96 - 712535 L	13:25	14:03	38 min
1290	72 - 80 - 8159502 L	13:25	15:27	122 min
1291	58 - 65 - 3125626 L	13:25	13:42	17 min
1292	80 - 112 - 4432667 L	13:25	14:53	88 min.
1293	59 - 66 - 3125627 L	13:25	13:49	24 min
1294	90 - 106 - 9133307 L	13:25	-	-
1295	81 - 113 - 4432668 L	13:26	14:19	53min
1296	36 - 54 - 1911266 L	13:26	-	-
1297	78 - 86 - 8159508 L	13:26	13:32	6 min.
1298	38 - 56 - 1911268 L	13:26	14:48	82 min.
1299	91 - 103 - 8231657 L	13:26	14:07	41 min
1300	65 - 72 - 3125633 L	13:27	15:29	122 min
1301	84 - 92 - 8159514 L	13:27	14:19	52 min
1302	79 - 86 - 3125647 L	13:27	14:08	41 min
1303	91 - 109 - 1911321 L	13:27	13:36	9 min.
1304	58 - 60 - 712499 L	13:28	15:34	126 min
1305	36 - 44 - 8159466 L	13:28	14:49	81 min.
1306	49 - 61 - 8231615 L	13:28	15:43	145 min
1307	54 - 70 - 9133271 L	13:29	13:51	22 min
1308	45 - 77 - 4432632 L	13:29	15:12	103 min
1309	90 - 108 - 1911320 L	13:30	14:05	35 min

No	Plate no	Arrival time	Dep. Time	duration
1310	8 - 10 - 712449 L	13:31	13:52	21 min
1311	88 - 99 - 139063 L	13:31	13:50	19 min
1312	75 - 91 - 3216963 L	13:31	14:42	71 min.
1313	4 - 20 - 9133221 L	13:32	14:16	44 min
1314	52 - 57 - 5984163 L	13:32	15:02	90 min
1315	14 - 42 - 7541145 L	13:32	13:03	101min
1316	89 - 107 - 1911319 L	13:32	-	-
1317	44 - 46 - 712485 L	13:32	14:21	49 min
1318	22 - 30 - 8159452 L	13:33	13:45	12 min
1319	35 - 47 - 8231601 L	13:33	14:50	77 min.
1320	9 - 16 - 3125577 L	13:34	13:41	7 min.
1321	31 - 63 - 4432618 L	13:35	14:30	55 min
1322	50 - 78 - 7541181 L	13:35	15:58	143 min
1323	69 - 71 - 712510 L	13:35	14:00	25 min
1324	60 - 72 - 8231626 L	13:36	15:08	92 min
1325	34 - 41 - 3125602 L	13:36	14:41	65 min.
1326	65 - 81 - 9133282 L	13:36	-	-
1327	87 - 105 - 1911317 L	13:36	14:24	48 min
1328	84 - 86 - 712525 L	13:36	14:44	8 min.
1329	62 - 70 - 8159492 L	13:37	14:19	42 min
1330	75 - 87 - 8231641 L	13:37	14:09	32 min
1331	49 - 56 - 3125617 L	13:37	14:31	54 min
1332	80 - 96 - 9133297 L	13:37	15:00	83 min.
1333	71 - 103 - 4432658 L	13:38	14:22	44 min
1334	26 - 44 - 1911256 L	13:38	13:47	9 min.
1335	90 - 118 - 7541221 L	13:38	-	-
1336	85 - 87 - 712526 L	13:38	15:05	87 min.
1337	18 - 36 - 1911248 L	13:39	14:08	29 min
1338	76 - 88 - 8231642 L	13:39	13:45	6 min.
1339	50 - 57 - 3125618 L	13:39	16:09	150 min
1340	81 - 97 - 9133298 L	13:39	-	-

No	Plate no	Arrival time	Dep. Time	duration
1341	72 - 104 - 4432659 L	13:40	13:47	7 min.
1342	82 - 110 - 7541213 L	13:40	15:16	96 min
1343	91 - 119 - 7541222 L	13:40	15:05	85 min.
1344	57 - 65 - 8159487 L	13:40	14:36	56 min
1345	70 - 82 - 8231636 L	13:41	13:50	9 min.
1346	44 - 51 - 3125612 L	13:41	14:05	24 min
1347	66 - 98 - 4432653 L	13:41	14:18	37 min
1348	21 - 39 - 1911251 L	13:41	14:24	43 min
1349	85 - 113 - 7541216 L	13:41	15:05	84 min.
1350	58 - 66 - 8159488 L	13:42	13:52	10 min
1351	71 - 83 - 8231637 L	13:42	13:50	8 min.
1352	45 - 52 - 3125613 L	13:42	16:01	139min
1353	76 - 92 - 9133293 L	13:42	15:15	93 min
1354	67 - 99 - 4432654 L	13:43	13:49	6 min.
1355	86 - 88 - 712527 L	13:43	14:06	23 min
1356	64 - 72 - 8159494 L	13:43	14:46	63 min.
1357	77 - 89 - 8231643 L	13:43	13:48	5 min.
1358	82 - 98 - 9133299 L	13:44	14:00	16 min
1359	73 - 105 - 4432660 L	13:44	14:42	58 min
1360	28 - 46 - 1911258 L	13:44	14:15	31 min
1361	87 - 89 - 712528 L	13:44	14:30	46 min
1362	65 - 73 - 8159495 L	13:45	14:52	67 min.
1363	78 - 90 - 8231644 L	13:45	14:25	40 min
1364	52 - 59 - 3125620 L	13:45	14:37	52 min
1365	83 - 99 - 9133300 L	13:45	13:53	8 min.
1366	74 - 106 - 4432661 L	13:45	15:46	121 min
1367	29 - 47 - 1911259 L	13:45	14:26	41 min
1368	86 - 114 - 7541217 L	13:45	14:13	28 min
1369	59 - 67 - 8159489 L	13:45	15:37	112 min
1370	72 - 84 - 8231638 L	13:46	13:58	12 min
1371	46 - 53 - 3125614 L	13:46	16:08	142 min

No	Plate no	Arrival time	Dep. Time	duration
1372	77 - 93 - 9133294 L	13:46	14:55	69 min.
1373	61 - 69 - 8159491 L	13:46	13:51	5 min.
1374	23 - 41 - 1911253 L	13:46	14:30	44 min
1375	87 - 115 - 7541218 L	13:46	13:53	7 min.
1376	73 - 85 - 8231639 L	13:46	14:12	26 min
1377	47 - 54 - 3125615 L	13:46	14:25	39 min
1378	78 - 94 - 9133295 L	13:46	14:42	56 min
1379	74 - 86 - 8231640 L	13:46	13:55	9 min.
1380	24 - 42 - 1911254 L	13:46	14:57	71 min.
1381	88 - 116 - 7541219 L	13:47	14:32	45 min
1382	48 - 55 - 3125616 L	13:47	14:16	29 min
1383	79 - 95 - 9133296 L	13:47	14:31	44 min
1384	89 - 117 - 7541220 L	13:47	15:05	78 min.
1385	67 - 75 - 8159497 L	13:47	13:55	8 min.
1386	85 - 101 - 9133302 L	13:47	14:19	32 min
1387	31 - 49 - 1911261 L	13:47	14:38	51 min
1388	68 - 76 - 8159498 L	13:47	-	-
1389	86 - 102 - 9133303 L	13:47	15:04	77 min
1390	77 - 109 - 4432664 L	13:48	14:22	34 min
1391	32 - 50 - 1911262 L	13:48	13:55	7 min.
1392	69 - 77 - 8159499 L	13:48	15:54	6 min.
1393	78 - 110 - 4432665 L	13:48	-	-
1394	33 - 51 - 1911263 L	13:48	14:25	37 min
1395	79 - 111 - 4432666 L	13:48	16:00	132 min
1396	34 - 52 - 1911264 L	13:48	14:54	66 min.
1397	13 - 33 - 1135503 L	13:49	14:07	18 min
1398	32 - 82 - 256456 L	13:49	14:36	47 min
1399	11 - 22 - 138986 L	13:49	15:06	77 min.
1400	14 - 34 - 1135504 L	13:49	14:09	20 min
1401	37 - 87 - 256461 L	13:49	13:57	8 min.
1402	15 - 26 - 138990 L	13:50	14:52	62 min

No	Plate no	Arrival time	Dep. Time	duration
1403	28 - 35 - 9125675 L	13:50	14:05	15 min
1404	33 - 83 - 256457 L	13:50	13:56	6 min.
1405	16 - 27 - 138991 L	13:51	15:43	112 min
1406	25 - 32 - 9125672 L	13:51	14:23	32 min
1407	8 - 34 - 4768918 L	13:51	15:13	88 min
1408	26 - 46 - 1135516 L	13:52	15:12	80 min.
1409	18 - 48 - 5611341 L	13:52	14:44	52 min
1410	37 - 53 - 2113684 L	13:52	14:00	8 min.
1411	65 - 81 - 2113712 L	13:53	14:34	41 min
1412	35 - 85 - 256459 L	13:53	14:19	26 min
1413	17 - 28 - 138992 L	13:53	13:59	6 min.
1414	26 - 33 - 9125673 L	13:53	15:56	123 min
1415	9 - 35 - 4768919 L	13:53	14:54	61 min
1416	27 - 47 - 1135517 L	13:54	15:35	101 min
1417	19 - 49 - 5611342 L	13:54	14:54	60 min.
1418	38 - 54 - 2113685 L	13:54	14:23	29 min
1419	66 - 82 - 2113713 L	13:54	14:00	6 min.
1420	36 - 86 - 256460 L	13:54	14:01	7 min.
1421	20 - 31 - 138995 L	13:54	14:05	11 min
1422	27 - 34 - 9125674 L	13:55	15:13	78 min
1423	10 - 36 - 4768920 L	13:55	16:08	133 min
1424	28 - 48 - 1135518 L	13:55	14:25	30 min
1425	22 - 52 - 5611345 L	13:56	-	-
1426	41 - 57 - 2113688 L	13:56	14:04	8 min.
1427	67 - 83 - 2113714 L	13:56	14:56	60 min
1428	44 - 94 - 256468 L	13:57	15:06	69 min
1429	21 - 32 - 138996 L	13:57	14:52	55 min
1430	33 - 40 - 9125680 L	13:57	14:37	40 min
1431	11 - 37 - 4768921 L	13:57	15:07	70 min.
1432	33 - 53 - 1135523 L	13:58	14:06	8 min.
1433	23 - 53 - 5611346 L	13:58	14:58	60 min

No	Plate no	Arrival time	Dep. Time	duration
1434	42 - 58 - 2113689 L	13:59	15:06	67 min.
1435	68 - 84 - 2113715 L	13:59	14:21	22 min
1436	45 - 95 - 256469 L	13:59	14:08	9 min.
1437	23 - 34 - 138998 L	14:00	15:32	92 min
1438	34 - 41 - 9125681 L	14:00	15:01	61 min.
1439	12 - 38 - 4768922 L	14:00	14:40	40 min
1440	34 - 54 - 1135524 L	14:01	14:21	20 min
1441	24 - 54 - 5611347 L	14:01	14:14	13 min
1442	43 - 59 - 2113690 L	14:01	14:07	6 min.
1443	69 - 85 - 2113716 L	14:01	16:05	124 min
1444	51 - 101 - 256475 L	14:02	15:13	71 min.
1445	34 - 45 - 139009 L	14:02	16:12	130 min
1446	42 - 49 - 9125689 L	14:02	14:32	30 min
1447	13 - 39 - 4768923 L	14:02	-	-
1448	35 - 55 - 1135525 L	14:03	14:56	53 min
1449	25 - 55 - 5611348 L	14:03	14:08	5 min.
1450	44 - 60 - 2113691 L	14:03	16:53	170 min
1451	70 - 86 - 2113717 L	14:03	15:25	82 min.
1452	52 - 102 - 256476 L	14:03	14:44	41 min
1453	37 - 48 - 139012 L	14:04	14:32	28 min
1454	43 - 50 - 9125690 L	14:04	15:46	102 min
1455	14 - 40 - 4768924 L	14:04	15:17	73 min.
1456	38 - 58 - 1135528 L	14:05	14:14	9 min.
1457	26 - 56 - 5611349 L	14:05	16:08	123 min
1458	45 - 61 - 2113692 L	14:05	14:39	34 min
1459	56 - 106 - 256480 L	14:05	14:50	45 min
1460	38 - 49 - 139013 L	14:06	14:24	18 min
1461	47 - 54 - 9125694 L	14:06	15:21	75 min.
1462	15 - 41 - 4768925 L	14:06	15:38	92 min
1463	39 - 59 - 1135529 L	14:06	15:05	59 min
1464	39 - 69 - 5611362 L	14:07	14:16	9 min.

No	Plate no	Arrival time	Dep. Time	duration
1465	46 - 62 - 2113693 L	14:07	16:00	113 min
1466	57 - 107 - 256481 L	14:07	14:12	5 min
1467	39 - 50 - 139014 L	14:07	14:24	17 min
1468	48 - 55 - 9125695 L	14:07	16:31	144 min
1469	16 - 42 - 4768926 L	14:08	16:00	112 min
1470	40 - 60 - 1135530 L	14:08	14:50	42 min
1471	47 - 77 - 5611370 L	14:08	14:46	38 min
1472	47 - 63 - 2113694 L	14:08	14:17	9 min.
1473	58 - 108 - 256482 L	14:08	15:35	87 min
1474	40 - 51 - 139015 L	14:09	16:11	122 min
1475	49 - 56 - 9125696 L	14:09	14:31	22 min
1476	17 - 43 - 4768927 L	14:09	15:16	67 min.
1477	15 - 35 - 1135505 L	14:09	15:07	58 min
1478	48 - 78 - 5611371 L	14:10	14:33	23 min
1479	48 - 64 - 2113695 L	14:10	14:57	47 min
1480	59 - 109 - 256483 L	14:10	14:42	32 min
1481	49 - 65 - 2113696 L	14:10	15:17	67 min..
1482	50 - 57 - 9125697 L	14:10	16:09	119 min
1483	18 - 44 - 4768928 L	14:11	15:20	9 min.
1484	17 - 37 - 1135507 L	14:11	-	-
1485	49 - 79 - 5611372 L	14:12	14:53	41 min
1486	60 - 110 - 256484 L	14:12	14:33	21 min
1487	50 - 66 - 2113697 L	14:12	14:31	19 min
1488	51 - 58 - 9125698 L	14:12	15:07	55 min
1489	20 - 46 - 4768930 L	14:12	14:51	39 min
1490	52 - 72 - 1135542 L	14:13	16:04	111 min
1491	50 - 80 - 5611373 L	14:13	14:20	7 min.
1492	51 - 67 - 2113698 L	14:13	16:26	133 min
1493	52 - 59 - 9125699 L	14:14	15:02	48 min
1494	17 - 33 - 3216905 L	14:14	15:59	105 min
1495	53 - 73 - 1135543 L	14:14	16:21	127 min

No	Plate no	Arrival time	Dep. Time	duration
1496	51 - 81 - 5611374 L	14:15	15:24	69 min.
1497	53 - 60 - 9125700 L	14:15	14:50	35 min
1498	21 - 37 - 3216909 L	14:15	14:43	27 min
1499	54 - 74 - 1135544 L	14:15	16:23	128 mi
1500	54 - 61 - 9125701 L	14:16	14:24	8 min.
1501	22 - 38 - 3216910 L	14:16	14:48	32 min
1502	55 - 75 - 1135545 L	14:16	15:07	51 min
1503	23 - 39 - 3216911 L	14:16	15:57	101 min
1504	56 - 76 - 1135546 L	14:17	16:03	46 min
1505	26 - 42 - 3216914 L	14:17	14:43	26 min
1506	12 - 43 - 1689426 L	14:17	14:53	36 min
1507	27 - 43 - 3216915 L	14:17	14:24	7 min
1508	63 - 113 - 256487 L	14:18	16:28	130 min
1509	44 - 55 - 139019 L	14:18	16:11	113 min
1510	58 - 65 - 9125705 L	14:18	14:26	8 min.
1511	31 - 47 - 3216919 L	14:18	16:24	126 min
1512	63 - 83 - 1135553 L	14:18	15:47	89 min
1513	53 - 83 - 5611376 L	14:19	15:13	54 min
1514	13 - 18 - 5984124 L	14:19	14:55	36 min
1515	79 - 95 - 2113726 L	14:19	14:30	11 min
1516	17 - 48 - 1689431 L	14:19	15:48	89 min.
1517	36 - 62 - 4768946 L	14:20	14:53	33 min
1518	84 - 134 - 256508 L	14:20	15:01	41 min
1519	45 - 56 - 139020 L	14:20	-	-
1520	71 - 78 - 9125718 L	14:20	14:27	7 min.
1521	32 - 48 - 3216920 L	14:20	15:58	98 min
1522	80 - 100 - 1135570 L	14:20	14:42	22 min
1523	54 - 84 - 5611377 L	14:21	16:15	114 min
1524	26 - 31 - 5984137 L	14:21	14:34	13 min
1525	80 - 96 - 2113727 L	14:21	16:52	151 min
1526	18 - 49 - 1689432 L	14:21	14:27	6 min.

No	Plate no	Arrival time	Dep. Time	duration
1527	37 - 63 - 4768947 L	14:21	15:52	91 min
1528	87 - 137 - 256511 L	14:22	15:06	44 min
1529	51 - 62 - 139026 L	14:22	15:04	42 min
1530	60 - 67 - 9125707 L	14:22	14:47	25 min
1531	45 - 61 - 3216933 L	14:22	14:28	6 min.
1532	65 - 85 - 1135555 L	14:23	14:33	10 min
1533	62 - 92 - 5611385 L	14:23	15:30	67 min.
1534	15 - 20 - 5984126 L	14:23	16:02	99 min
1535	87 - 103 - 2113734 L	14:23	15:44	81 min
1536	34 - 65 - 1689448 L	14:23	14:59	36 min
1537	50 - 76 - 4768960 L	14:24	14:31	7 min.
1538	89 - 139 - 256513 L	14:24	15:12	48 min
1539	52 - 63 - 139027 L	14:24	14:30	6 min.
1540	66 - 73 - 9125713 L	14:24	15:09	45 min
1541	34 - 50 - 3216922 L	14:24	15:17	53 min
1542	72 - 92 - 1135562 L	14:25	16:28	123 min
1543	63 - 93 - 5611386 L	14:25	14:33	8 min.
1544	16 - 21 - 5984127 L	14:25	16:20	115 min
1545	88 - 104 - 2113735 L	14:26	15:04	38 min
1546	20 - 51 - 1689434 L	14:26	15:26	60 min.
1547	51 - 77 - 4768961 L	14:26	14:41	15 min
1548	9 - 11 - 712450 L	14:26	14:54	28 min
1549	65 - 76 - 139040 L	14:27	15:04	37 min
1550	67 - 74 - 9125714 L	14:27	14:32	5 min.
1551	41 - 57 - 3216929 L	14:27	15:16	49 min
1552	73 - 93 - 1135563 L	14:27	15:32	65 min
1553	64 - 94 - 5611387 L	14:28	15:40	72 min
1554	17 - 22 - 5984128 L	14:28	16:28	120 min
1555	83 - 99 - 2113730 L	14:28	14:36	8 min.
1556	21 - 52 - 1689435 L	14:28	15:25	57 min
1557	46 - 72 - 4768956 L	14:29	14:35	6 min.

No	Plate no	Arrival time	Dep. Time	duration
1558	13 - 15 - 712454 L	14:29	14:42	13 min
1559	69 - 80 - 139044 L	14:29	15:10	41 min
1560	68 - 75 - 9125715 L	14:29	15:02	33 min
1561	42 - 58 - 3216930 L	14:30	14:36	6 min.
1562	74 - 94 - 1135564 L	14:30	15:47	77 min
1563	67 - 97 - 5611390 L	14:30	16:31	121 min
1564	18 - 23 - 5984129 L	14:30	15:30	60 min.
1565	91 - 107 - 2113738 L	14:31	14:49	18 min
1566	38 - 69 - 1689452 L	14:31	14:39	8 min.
1567	47 - 73 - 4768957 L	14:31	14:54	23 min
1568	14 - 16 - 712455 L	14:31	16:03	92 min
1569	70 - 81 - 139045 L	14:31	16:49	138 min
1570	72 - 79 - 9125719 L	14:32	15:50	78 min.
1571	43 - 59 - 3216931 L	14:32	16:25	113 min
1572	81 - 101 - 1135571 L	14:32	14:41	9 min.
1573	69 - 99 - 5611392 L	14:32	15:26	54 min
1574	29 - 34 - 5984140 L	14:33	15:12	39 min
1575	93 - 109 - 2113740 L	14:33	15:18	45 min
1576	23 - 54 - 1689437 L	14:33	15:50	77 min
1577	48 - 74 - 4768958 L	14:34	-	-
1578	15 - 17 - 712456 L	14:34	15:59	85 min
1579	71 - 82 - 139046 L	14:34	14:40	6 min.
1580	75 - 82 - 9125722 L	14:34	15:19	45 min
1581	46 - 62 - 3216934 L	14:35	14:57	22 min
1582	83 - 103 - 1135573 L	14:35	15:39	64 min.
1583	74 - 104 - 5611397 L	14:35	14:46	11 min
1584	20 - 25 - 5984131 L	14:35	15:52	77 min.
1585	94 - 110 - 2113741 L	14:36	17:06	150 min
1586	25 - 56 - 1689439 L	14:36	14:45	9 min.
1587	57 - 83 - 4768967 L	14:36	15:09	33 min
1588	17 - 19 - 712458 L	14:36	16:13	97 min

No	Plate no	Arrival time	Dep. Time	duration
1589	72 - 83 - 139047 L	14:37	14:45	8 min.
1590	76 - 83 - 9125723 L	14:37	16:48	131 min
1591	53 - 69 - 3216941 L	14:37	15:28	51 min
1592	84 - 104 - 1135574 L	14:37	15:56	79 min.
1593	75 - 105 - 5611398 L	14:38	14:45	7 min.
1594	30 - 35 - 5984141 L	14:38	16:27	49 min
1595	95 - 111 - 2113742 L	14:38	14:47	9 min
1596	27 - 58 - 1689441 L	14:39	14:55	16 min
1597	58 - 84 - 4768968 L	14:39	15:44	65 min.
1598	18 - 20 - 712459 L	14:39	15:02	23 min
1599	75 - 86 - 139050 L	14:39	16:40	121 min
1600	78 - 85 - 9125725 L	14:40	14:48	8 min.
1601	54 - 70 - 3216942 L	14:40	15:40	60 min.
1602	85 - 105 - 1135575 L	14:42	15:09	27 min
1603	79 - 109 - 5611402 L	14:42	14:50	8 min.
1604	22 - 27 - 5984133 L	14:43	15:28	45 min
1605	98 - 114 - 2113745 L	14:43	15:20	37 min
1606	28 - 59 - 1689442 L	14:44	16:03	79 min.
1607	60 - 86 - 4768970 L	14:45	16:34	109 min
1608	19 - 21 - 712460 L	14:45	-	-
1609	76 - 87 - 139051 L	14:45	14:54	9 min.
1610	79 - 86 - 9125726 L	14:46	16:27	101 min
1611	56 - 72 - 3216944 L	14:46	15:44	58 min
1612	87 - 107 - 1135577 L	14:47	15:54	67 min.
1613	80 - 110 - 5611403 L	14:47	16:25	98 min
1614	23 - 28 - 5984134 L	14:48	16:23	95 min
1615	99 - 115 - 2113746 L	14:49	15:08	19 min
1616	35 - 66 - 1689449 L	14:50	14:58	8 min.
1617	61 - 87 - 4768971 L	14:51	15:38	47 min
1618	22 - 24 - 712463 L	14:51	14:56	5 min.
1619	77 - 88 - 139052 L	14:52	15:44	52 min

No	Plate no	Arrival time	Dep. Time	duration
1620	80 - 87 - 9125727 L	14:52	15:16	24 min
1621	58 - 74 - 3216946 L	14:53	16:22	89 min.
1622	90 - 110 - 1135580 L	14:54	15:01	7 min.
1623	84 - 114 - 5611407 L	14:55	15:15	20mn
1624	24 - 29 - 5984135 L	14:56	15:03	9 min.
1625	5 - 33 - 7541136 L	14:57	17:09	132 min
1626	36 - 67 - 1689450 L	14:59	15:42	43 min
1627	62 - 88 - 4768972 L	14:59	-	-
1628	27 - 29 - 712468 L	15:00	15:55	55 min
1629	84 - 95 - 139059 L	15:00	15:08	8 min
1630	82 - 89 - 9125729 L	15:01	16:09	68 min.
1631	59 - 75 - 3216947 L	15:01	15:10	9 min.
1632	95 - 115 - 1135585 L	15:02	15:14	12 min
1633	85 - 115 - 5611408 L	15:02	15:10	8 min.
1634	34 - 39 - 5984145 L	15:03	15:08	5 min.
1635	6 - 34 - 7541137 L	15:04	15:31	27 min
1636	47 - 78 - 1689461 L	15:04	16:21	77 min.
1637	64 - 90 - 4768974 L	15:05	15:12	7 min.
1638	29 - 31 - 712470 L	15:05	15:27	22 min
1639	85 - 96 - 139060 L	15:06	16:47	101 min
1640	83 - 90 - 9125730 L	15:07	16:35	88 min.
1641	62 - 78 - 3216950 L	15:07	17:00	113 min
1642	98 - 118 - 1135588 L	15:08	-	-
1643	86 - 116 - 5611409 L	15:09	15:17	8 min.
1644	39 - 44 - 5984150 L	15:10	16:10	60 min
1645	7 - 35 - 7541138 L	15:11	16:49	99 min
1646	48 - 79 - 1689462 L	15:12	15:17	5 min
1647	65 - 91 - 4768975 L	15:13	15:31	18 min
1648	30 - 32 - 712471 L	15:13	15:46	33 min
1649	86 - 97 - 139061 L	15:14	15:20	6 min.
1650	84 - 91 - 9125731 L	15:14	16:21	67 min

No	Plate no	Arrival time	Dep. Time	duration
1651	63 - 79 - 3216951 L	15:15	15:34	19 min
1652	9 - 25 - 9133226 L	15:16	15:24	8 min.
1653	88 - 118 - 5611411 L	15:17	16:42	85 min
1654	40 - 45 - 5984151 L	15:18	16:00	42 min
1655	8 - 36 - 7541139 L	15:20	17:18	118 min
1656	51 - 82 - 1689465 L	15:20	15:28	8 min.
1657	67 - 93 - 4768977 L	15:21	16:07	46 min
1658	33 - 35 - 712474 L	15:21	16:27	66 min
1659	90 - 101 - 139065 L	15:21	17:01	100 min
1660	88 - 95 - 9125735 L	15:22	16:00	38 min
1661	67 - 83 - 3216955 L	15:22	15:44	22 min
1662	10 - 26 - 9133227 L	15:23	15:30	7 min.
1663	89 - 119 - 5611412 L	15:24	16:26	62 min
1664	41 - 46 - 5984152 L	15:25	15:34	9 min.
1665	9 - 37 - 7541140 L	15:26	16:37	71 min.
1666	52 - 83 - 1689466 L	15:26	16:39	73 min
1667	70 - 96 - 4768980 L	15:26	16:34	68 min
1668	34 - 36 - 712475 L	15:27	17:45	138 min
1669	94 - 105 - 139069 L	15:28	15:33	5 min.
1670	97 - 104 - 9125744 L	15:28	16:07	39 min
1671	69 - 85 - 3216957 L	15:29	17:01	92 min
1672	11 - 27 - 9133228 L	15:29	15:53	24 min
1673	90 - 120 - 5611413 L	15:29	16:22	53 min
1674	43 - 48 - 5984154 L	15:30	17:24	114 min
1675	10 - 38 - 7541141 L	15:30	16:48	78 min
1676	53 - 84 - 1689467 L	15:31	16:23	52 min
1677	71 - 97 - 4768981 L	15:31	15:39	8 min.
1678	35 - 37 - 712476 L	15:32	15:58	26 min
1679	95 - 106 - 139070 L	15:33	15:49	16 min
1680	98 - 105 - 9125745 L	15:33	15:40	7 min.
1681	70 - 86 - 3216958 L	15:34	16:09	35 min

No	Plate no	Arrival time	Dep. Time	duration
1682	12 - 28 - 9133229 L	15:35	16:51	76 min.
1683	94 - 124 - 5611417 L	15:36	15:41	5 min.
1684	44 - 49 - 5984155 L	15:37	16:25	48 min
1685	11 - 39 - 7541142 L	15:37	-	-
1686	55 - 86 - 1689469 L	15:38	16:13	35 min
1687	80 - 106 - 4768990 L	15:39	16:01	22 min
1688	36 - 38 - 712477 L	15:42	16:48	66 min.
1689	96 - 107 - 139071 L	15:44	15:50	6 min.
1690	99 - 106 - 9125746 L	15:45	17:24	99 min
1691	71 - 87 - 3216959 L	15:46	18:10	144 min
1692	13 - 29 - 9133230 L	15:47	17:05	88 min
1693	3 - 35 - 4432590 L	15:48	17:51	123 min
1694	45 - 50 - 5984156 L	15:49	15:56	7 min
1695	12 - 40 - 7541143 L	15:50	18:06	136 min
1696	56 - 87 - 1689470 L	15:51	16:32	41 min
1697	81 - 107 - 4768991 L	15:52	16:00	8 min.
1698	45 - 47 - 712486 L	15:54	17:53	119 min
1699	97 - 108 - 139072 L	15:56	16:51	55 min
1700	2 - 14 - 8231568 L	15:56	16:02	6 min.
1701	72 - 88 - 3216960 L	15:57	16:28	31 min
1702	14 - 30 - 9133231 L	15:58	17:59	121 min
1703	5 - 37 - 4432592 L	15:59	16:50	51 min
1704	46 - 51 - 5984157 L	15:59	16:28	29 min
1705	20 - 48 - 7541151 L	16:00	16:12	12 min
1706	57 - 88 - 1689471 L	16:01	16:06	5 min.
1707	85 - 111 - 4768995 L	16:03	18:23	140 min
1708	46 - 48 - 712487 L	16:05	17:33	88 min.
1709	98 - 109 - 139073 L	16:05	-	-
1710	4 - 16 - 8231570 L	16:06	16:13	7 min.
1711	73 - 89 - 3216961 L	16:08	17:52	104 min
1712	15 - 31 - 9133232 L	16:09	17:39	90 min

No	Plate no	Arrival time	Dep. Time	duration
1713	6 - 38 - 4432593 L	16:10	16:18	8 min.
1714	47 - 52 - 5984158 L	16:10	16:49	39 min
1715	23 - 51 - 7541154 L	16:11	17:50	99 min
1716	65 - 96 - 1689479 L	16:14	17:12	58 min
1717	88 - 114 - 4768998 L	16:14	18:12	118 min
1718	47 - 49 - 712488 L	16:19	16:43	24 min
1719	7 - 15 - 8159437 L	16:19	16:37	18 min
1720	5 - 17 - 8231571 L	16:20	16:29	9 min.
1721	82 - 98 - 3216970 L	16:21	18:41	140 min
1722	16 - 32 - 9133233 L	16:22	17:24	62 min
1723	7 - 39 - 4432594 L	16:23	16:39	16 min
1724	48 - 53 - 5984159 L	16:24	-	-
1725	24 - 52 - 7541155 L	16:25	16:58	33 min
1726	66 - 97 - 1689480 L	16:28	16:37	9 min.
1727	89 - 115 - 4768999 L	16:29	17:43	74 min
1728	48 - 50 - 712489 L	16:30	18:44	144 min
1729	8 - 16 - 8159438 L	16:31	16:37	6 min
1730	6 - 18 - 8231572 L	16:32	16:41	9 min.
1731	83 - 99 - 3216971 L	16:33	17:50	77 min.
1732	21 - 37 - 9133238 L	16:35	17:28	53 min
1733	11 - 43 - 4432598 L	16:36	16:58	22 min
1734	54 - 59 - 5984165 L	16:37	16:42	5 min.
1735	26 - 54 - 7541157 L	16:37	18:52	135 min
1736	67 - 98 - 1689481 L	16:38	17:38	60 min
1737	91 - 117 - 4769001 L	16:40	18:49	129 min
1738	49 - 51 - 712490 L	16:40	-	-
1739	10 - 18 - 8159440 L	16:41	16:49	8 min.
1740	13 - 25 - 8231579 L	16:42	17:59	77 min
1741	84 - 100 - 3216972 L	16:43	18:48	125 min
1742	22 - 38 - 9133239 L	16:44	16:49	5 min.
1743	12 - 44 - 4432599 L	16:45	17:25	40 min

No	Plate no	Arrival time	Dep. Time	duration
1744	55 - 60 - 5984166 L	16:47	16:53	6 min.
1745	28 - 56 - 7541159 L	16:49	17:08	19 min
1746	69 - 100 - 1689483 L	16:50	17:57	67 Min.
1747	92 - 118 - 4769002 L	16:51	16:56	5 min.
1748	50 - 52 - 712491 L	16:52	17:14	22 min
1749	11 - 19 - 8159441 L	16:53	17:40	47 min
1750	14 - 26 - 8231580 L	16:53	17:03	10 min
1751	85 - 101 - 3216973 L	16:54	17:01	7 min.
1752	24 - 40 - 9133241 L	16:54	17:49	55 min
1753	13 - 45 - 4432600 L	16:54	17:24	30 min
1754	57 - 62 - 5984168 L	16:55	17:34	99 min
1755	29 - 57 - 7541160 L	16:56	17:04	8 min.
1756	70 - 101 - 1689484 L	16:57	17:29	32 min
1757	94 - 120 - 4769004 L	16:57	18:37	100 min
1758	51 - 53 - 712492 L	16:58	17:06	8 min.
1759	12 - 20 - 8159442 L	16:58	17:59	61 min.
1760	19 - 31 - 8231585 L	16:59	18:50	111 min
1761	86 - 102 - 3216974 L	17:00	18:17	77 min
1762	25 - 41 - 9133242 L	17:01	17:06	5 min.
1763	20 - 52 - 4432607 L	17:01	18:20	79 min.
1764	60 - 65 - 5984171 L	17:02	18:39	97 min
1765	34 - 62 - 7541165 L	17:02	18:05	63 min
1766	71 - 102 - 1689485 L	17:03	17:26	23 min
1767	54 - 56 - 712495 L	17:04	17:20	16 min
1768	14 - 22 - 8159444 L	17:04	17:11	7 min.
1769	21 - 33 - 8231587 L	17:05	18:18	73 min
1770	87 - 103 - 3216975 L	17:05	-	-
1771	26 - 42 - 9133243 L	17:06	18:11	65 min
1772	21 - 53 - 4432608 L	17:06	18:34	88 min.
1773	61 - 66 - 5984172 L	17:07	-	-
1774	35 - 63 - 7541166 L	17:07	17:32	25 min

No	Plate no	Arrival time	Dep. Time	duration
1775	72 - 103 - 1689486 L	17:08	17:16	8 min.
1776	55 - 57 - 712496 L	17:08	18:12	64 min.
1777	15 - 23 - 8159445 L	17:09	18:23	74 min
1778	23 - 35 - 8231589 L	17:09	17:47	38 min
1779	89 - 105 - 3216977 L	17:10	17:19	9 min.
1780	28 - 44 - 9133245 L	17:11	17:46	35 min
1781	22 - 54 - 4432609 L	17:11	18:29	78 min.
1782	62 - 67 - 5984173 L	17:11	-	-
1783	36 - 64 - 7541167 L	17:12	17:18	6 min.
1784	73 - 104 - 1689487 L	17:12	18:29	77 min
1785	59 - 61 - 712500 L	17:13	18:08	55 min
1786	20 - 28 - 8159450 L	17:13	18:44	91 min
1787	24 - 36 - 8231590 L	17:14	17:20	6 min.
1788	90 - 106 - 3216978 L	17:14	18:31	79 min.
1789	29 - 45 - 9133246 L	17:15	17:48	33 min
1790	23 - 55 - 4432610 L	17:15	17:44	29 min
1791	63 - 68 - 5984174 L	17:16	18:07	51 min
1792	38 - 66 - 7541169 L	17:17	17:25	8 min
1793	74 - 105 - 1689488 L	17:17	17:39	22 min
1794	60 - 62 - 712501 L	17:17	18:45	88 min
1795	23 - 31 - 8159453 L	17:18	18:32	74 min
1796	32 - 44 - 8231598 L	17:20	18:50	90 min
1797	99 - 115 - 3216987 L	17:20	17:29	9 min.
1798	30 - 46 - 9133247 L	17:20	-	-
1799	24 - 56 - 4432611 L	17:21	18:43	82 min
1800	64 - 69 - 5984175 L	17:22	18:26	64 min
1801	39 - 67 - 7541170 L	17:23	17:29	6 min
1802	75 - 106 - 1689489 L	17:23	18:06	43 min
1803	61 - 63 - 712502 L	17:24	18:19	55 min
1804	24 - 32 - 8159454 L	17:25	17:32	7 min.
1805	33 - 45 - 8231599 L	17:25	17:40	15 min

No	Plate no	Arrival time	Dep. Time	duration
1806	4 - 11 - 3125572 L	17:26	-	-
1807	31 - 47 - 9133248 L	17:26	18:38	72min
1808	26 - 58 - 4432613 L	17:27	18:27	60 min
1809	69 - 74 - 5984180 L	17:27	18:08	41 min
1810	40 - 68 - 7541171 L	17:28	17:36	8 min.
1811	76 - 107 - 1689490 L	17:29	18:39	70 min
1812	62 - 64 - 712503 L	17:30	18:25	55 min
1813	25 - 33 - 8159455 L	17:31	18:37	66 min.
1814	36 - 48 - 8231602 L	17:32	18:15	43 min
1815	7 - 14 - 3125575 L	17:32	17:58	26 min
1816	32 - 48 - 9133249 L	17:33	18:11	38 min
1817	27 - 59 - 4432614 L	17:34	18:31	57 min
1818	70 - 75 - 5984181 L	17:35	-	-
1819	41 - 69 - 7541172 L	17:35	18:03	28 min
1820	80 - 111 - 1689494 L	17:36	18:47	11 min
1821	63 - 65 - 712504 L	17:37	18:47	70 min
1822	28 - 36 - 8159458 L	17:37	17:45	8 min.
1823	37 - 49 - 8231603 L	17:38	17:43	5 min.
1824	8 - 15 - 3125576 L	17:39	18:39	60 min.
1825	33 - 49 - 9133250 L	17:40	18:43	63 min
1826	28 - 60 - 4432615 L	17:41	-	-
1827	74 - 79 - 5984185 L	17:41	17:50	9 min.
1828	42 - 70 - 7541173 L	17:42	18:50	68 min.
1829	81 - 112 - 1689495 L	17:42	17:57	15 min
1830	64 - 66 - 712505 L	17:43	18:55	72 min.
1831	29 - 37 - 8159459 L	17:44	18:25	41 min
1832	38 - 50 - 8231604 L	17:44	18:33	49 min
1833	13 - 20 - 3125581 L	17:45	17:51	6 min.
1834	35 - 51 - 9133252 L	17:46	-	-
1835	29 - 61 - 4432616 L	17:46	18:49	63 min.
1836	76 - 81 - 5984187 L	17:47	18:22	35 min

No	Plate no	Arrival time	Dep. Time	duration
1837	43 - 71 - 7541174 L	17:47	18:11	24 min
1838	82 - 113 - 1689496 L	17:48	18:32	44 min
1839	68 - 70 - 712509 L	17:49	17:55	6 min.
1840	30 - 38 - 8159460 L	17:50	18:16	26 min
1841	42 - 54 - 8231608 L	17:51	18:53	62 min.
1842	14 - 21 - 3125582 L	17:51	18:48	57 min
1843	36 - 52 - 9133253 L	17:51	18:38	47 min
1844	30 - 62 - 4432617 L	17:52	18:54	62 min
1845	77 - 82 - 5984188 L	17:52	17:59	7 min.
1846	45 - 73 - 7541176 L	17:53	18:41	48 min
1847	84 - 115 - 1689498 L	17:53	18:58	65 min.
1848	70 - 72 - 712511 L	17:54	-	-
1849	33 - 41 - 8159463 L	17:55	18:01	6 min.
1850	43 - 55 - 8231609 L	17:56	18:57	61 min.
1851	15 - 22 - 3125583 L	17:57	-	-
1852	37 - 53 - 9133254 L	17:57	18:04	7 min.
1853	32 - 64 - 4432619 L	17:58	18:09	11 min
1854	78 - 83 - 5984189 L	17:59	18:41	42 min
1855	49 - 77 - 7541180 L	18:00	18:05	5 min.
1856	85 - 116 - 1689499 L	18:00	18:11	11 min
1857	71 - 73 - 712512 L	18:00	18:50	50 min
1858	37 - 45 - 8159467 L	18:00	18:09	9 min.
1859	44 - 56 - 8231610 L	18:01	-	-
1860	17 - 24 - 3125585 L	18:01	18:08	7 min.
1861	38 - 54 - 9133255 L	18:01	-	-
1862	33 - 65 - 4432620 L	18:02	18:55	53 min
1863	79 - 84 - 5984190 L	18:02	18:56	54 min
1864	52 - 80 - 7541183 L	18:02	18:10	8 min.
1865	86 - 117 - 1689500 L	18:02	18:23	21 min
1866	74 - 76 - 712515 L	18:03	-	-
1867	38 - 46 - 8159468 L	18:03	18:50	47 min

No	Plate no	Arrival time	Dep. Time	duration
1868	45 - 57 - 8231611 L	18:03	-	-
1869	18 - 25 - 3125586 L	18:03	18:10	7 min.
1870	41 - 57 - 9133258 L	18:04	18:13	9 min.
1871	34 - 66 - 4432621 L	18:04	18:53	49 min
1872	81 - 86 - 5984192 L	18:04	18:12	8 min.
1873	53 - 81 - 7541184 L	18:05	18:48	43 min
1874	88 - 119 - 1689502 L	18:05	18:36	31 min
1875	40 - 48 - 8159470 L	18:05	18:34	29 min
1876	46 - 58 - 8231612 L	18:06	18:14	8 min.
1877	24 - 31 - 3125592 L	18:06	18:30	24 min
1878	42 - 58 - 9133259 L	18:06	18:13	7 min
1879	35 - 67 - 4432622 L	18:07	18:22	15 min
1880	83 - 88 - 5984194 L	18:07	18:47	40 min
1881	55 - 83 - 7541186 L	18:07	18:12	5 min.
1882	93 - 124 - 1689507 L	18:07	-	-
1883	42 - 50 - 8159472 L	18:08	18:49	41 min
1884	50 - 62 - 8231616 L	18:08	-	-
1885	25 - 32 - 3125593 L	18:08	18:26	18 min
1886	43 - 59 - 9133260 L	18:08	18:14	6 min.
1887	36 - 68 - 4432623 L	18:09	18:31	22 min
1888	89 - 94 - 5984200 L	18:09	-	-
1889	57 - 85 - 7541188 L	18:09	18:53	44 min
1890	45 - 53 - 8159475 L	18:09	18:40	31 min
1891	51 - 63 - 8231617 L	18:10	18:19	9 min.
1892	26 - 33 - 3125594 L	18:10	18:36	26 min
1893	44 - 60 - 9133261 L	18:11	-	-
1894	38 - 70 - 4432625 L	18:11	18:44	33 min
1895	91 - 96 - 5984202 L	18:11	18:18	7 min.
1896	58 - 86 - 7541189 L	18:12	18:34	22 min
1897	49 - 57 - 8159479 L	18:13	-	--
1898	52 - 64 - 8231618 L	18:13	18:18	5 min

No	Plate no	Arrival time	Dep. Time	duration
1899	32 - 39 - 3125600 L	18:13	18:24	11 min
1900	45 - 61 - 9133262 L	18:14	18:47	33 min
1901	42 - 74 - 4432629 L	18:15	18:59	44 min
1902	92 - 97 - 5984203 L	18:16	18:24	8 min.
1903	59 - 87 - 7541190 L	18:16	18:58	42 min
1904	50 - 58 - 8159480 L	18:17	18:50	33 min
1905	53 - 65 - 8231619 L	18:17	18:46	29 min
1906	33 - 40 - 3125601 L	18:17	-	--
1907	53 - 69 - 9133270 L	18:17	18:22	5 min.
1908	46 - 78 - 4432633 L	18:18	18:56	38 min
1909	95 - 100 - 5984206 L	18:18	18:58	40 min
1910	65 - 93 - 7541196 L	18:18	18:17	9 min.
1911	51 - 59 - 8159481 L	18:18	18:35	17 min
1912	54 - 66 - 8231620 L	18:19	18:59	40 min
1913	35 - 42 - 3125603 L	18:19	18:32	13 min
1914	55 - 71 - 9133272 L	18:19	18:26	7 min.
1915	47 - 79 - 4432634 L	18:19	18:41	22 min
1916	96 - 101 - 5984207 L	18:20	18:53	33 min
1917	66 - 94 - 7541197 L	18:20	18:57	37 min
1918	52 - 60 - 8159482 L	18:20	18:42	22 min
1919	55 - 67 - 8231621 L	18:20	18:28	8 min.
1920	36 - 43 - 3125604 L	18:21	18:56	35 min
1921	56 - 72 - 9133273 L	18:21	18:49	28 min
1922	48 - 80 - 4432635 L	18:21	-	-
1923	2 - 20 - 1911232 L	18:22	18:31	9 min.
1924	67 - 95 - 7541198 L	18:22	18:28	6 min.
1925	79 - 87 - 8159509 L	18:22	18:43	21 min
1926	58 - 70 - 8231624 L	18:22	18:45	25 min
1927	37 - 44 - 3125605 L	18:23	18:57	34 min.
1928	57 - 73 - 9133274 L	18:23	18:32	9 min.
1929	49 - 81 - 4432636 L	18:23	18:41	18 min

No	Plate no	Arrival time	Dep. Time	duration
1930	3 - 21 - 1911233 L	18:23	18:53	30 min
1931	68 - 96 - 7541199 L	18:24	-	-
1932	80 - 88 - 8159510 L	18:24	18:33	9 min.
1933	59 - 71 - 8231625 L	18:24	18:50	26 min
1934	38 - 45 - 3125606 L	18:24	18:31	7 min.
1935	58 - 74 - 9133275 L	18:24	18:47	23 min
1936	51 - 83 - 4432638 L	18:25	-	-
1937	11 - 29 - 1911241 L	18:25	-	-
1938	70 - 98 - 7541201 L	18:25	18:42	17 min
1939	86 - 94 - 8159516 L	18:25	18:36	11 min
1940	61 - 73 - 8231627 L	18:26	18:32	6 min.
1941	39 - 46 - 3125607 L	18:26	18:34	8 min.
1942	60 - 76 - 9133277 L	18:26	18:58	32 min
1943	54 - 86 - 4432641 L	18:27	18:43	16 min
1944	12 - 30 - 1911242 L	18:27	-	-
1945	73 - 101 - 7541204 L	18:27	18:45	18 min
1946	87 - 95 - 8159517 L	18:27	18:34	7 min.
1947	62 - 74 - 8231628 L	18:28	18:53	25 min
1948	62 - 69 - 3125630 L	18:28	18:37	9 min.
1949	63 - 79 - 9133280 L	18:28	18:36	8 min
1950	55 - 87 - 4432642 L	18:28	-	-
1951	15 - 33 - 1911245 L	18:29	18:52	23 min
1952	75 - 103 - 7541206 L	18:29	18:37	8 min.
1953	88 - 96 - 8159518 L	18:29	18:34	5 min.
1954	63 - 75 - 8231629 L	18:29	18:56	27 min
1955	63 - 70 - 3125631 L	18:29	-	-
1956	64 - 80 - 9133281 L	18:30	18:55	25 min
1957	56 - 88 - 4432643 L	18:30	18:35	5 min.
1958	16 - 34 - 1911246 L	18:30	-	-
1959	76 - 104 - 7541207 L	18:30	18:59	29 min
1960	89 - 97 - 8159519 L	18:31	-	-

No	Plate no	Arrival time	Dep. Time	duration
1961	64 - 76 - 8231630 L	18:31	18:58	27 min
1962	66 - 73 - 3125634 L	18:31	18:38	7 min.
1963	66 - 82 - 9133283 L	18:31	18:53	22 min.
1964	57 - 89 - 4432644 L	18:32	18:38	6 min
1965	39 - 57 - 1911269 L	18:32	18:58	26 min
1966	77 - 105 - 7541208 L	18:32	-	-
1967	90 - 98 - 8159520 L	18:32	18:39	7 min
1968	88 - 100 - 8231654 L	18:33	18:57	24 min
1969	67 - 74 - 3125635 L	18:33	18:59	26 min
1970	67 - 83 - 9133284 L	18:33	18:55	22 min
1971	60 - 92 - 4432647 L	18:33	18:42	9 min
1972	40 - 58 - 1911270 L	18:34	18:39	5 min.
1973	79 - 107 - 7541210 L	18:34	18:46	12 min
1974	91 - 99 - 8159521 L	18:34	18:54	20 min
1975	89 - 101 - 8231655 L	18:34	18:58	24 min
1976	68 - 75 - 3125636 L	18:35	18:41	6 min
1977	68 - 84 - 9133285 L	18:35	18:58	23 min
1978	61 - 93 - 4432648 L	18:35	18:56	21 min
1979	43 - 61 - 1911273 L	18:35	18:52	17 min
1980	80 - 108 - 7541211 L	18:36	-	-
1981	92 - 100 - 8159522 L	18:36	18:52	16 min
1982	92 - 104 - 8231658 L	18:36	18:42	6 min
1983	69 - 76 - 3125637 L	18:36	18:54	18 min
1984	69 - 85 - 9133286 L	18:37	18:48	11 min
1985	84 - 116 - 4432671 L	18:37	18:57	20 min
1986	44 - 62 - 1911274 L	18:37	18:50	13 min
1987	93 - 101 - 8159523 L	18:38	18:53	15 min
1988	70 - 77 - 3125638 L	18:38	18:59	21 min
1989	70 - 86 - 9133287 L	18:38	18:44	6 min
1990	85 - 117 - 4432672 L	18:38	-	-
1991	46 - 64 - 1911276 L	18:39	18:57	18 min

No	Plate no	Arrival time	Dep. Time	duration
1992	59 - 70 - 139034 L	18:41	18:56	15 min
1993	39 - 65 - 4768949 L	18:41	18:50	9 min.
1994	61 - 68 - 9125708 L	18:42	-	-
1995	41 - 61 - 1135531 L	18:43	18:56	13 min
1996	58 - 74 - 2113705 L	18:44	18:54	10 min
1997	60 - 71 - 139035 L	18:45	18:56	11 min
1998	42 - 72 - 5611365 L	18:46	18:54	8 min
1999	62 - 78 - 2113709 L	18:46	-	-
2000	96 - 98 - 712537 L	18:47	-	-
2001	42 - 73 - 1689456 L	18:47	18:53	6 min
2002	61 - 72 - 139036 L	18:47	-	-
2003	43 - 74 - 1689457 L	18:48	-	-
2004	53 - 79 - 4768963 L	18:49	18:58	9 min
2005	1 - 51 - 256425 L	18:50	18:55	5 min
2006	30 - 56 - 4768940 L	18:50	-	-
2007	5 - 21 - 3216893 L	18:51	-	-
2008	73 - 80 - 3125641 L	18:51	18:56	5 min,
2009	94 - 110 - 9133311 L	18:52	-	-
2010	89 - 121 - 4432676 L	18:52	-	-
2011	49 - 67 - 1911279 L	18:52	-	-
2012	31 - 36 - 5984142 L	18:52	18:59	7 min
2013	50 - 70 - 1135540 L	18:53	-	-
2014	50 - 66 - 3216938 L	18:53	18:59	6 min
2015	31 - 38 - 9125678 L	18:54	-	-
2016	42 - 92 - 256466 L	18:55	-	-
2017	57 - 68 - 139032 L	18:55	-	-
2018	74 - 81 - 3125642 L	18:55	-	-
2019	90 - 122 - 4432677 L	18:56	-	-
2020	50 - 68 - 1911280 L	18:56	-	-
2021	13 - 41 - 7541144 L	18:56	-	-
2022	99 - 130 - 1689513 L	18:56	-	-

No	Plate no	Arrival time	Dep. Time	duration
2023	90 - 116 - 4769000 L	18:57	-	-
2024	23 - 25 - 712464 L	18:57	-	-
2025	48 - 56 - 8159478 L	18:58	-	-
2026	26 - 38 - 8231592 L	18:58	-	-
2027	51 - 58 - 3125619 L	18:58	-	-
2028	76 - 83 - 3125644 L	18:59	-	-
2029	92 - 124 - 4432679 L	18:59	-	-
2030	54 - 72 - 1911284 L	18:59	-	-
2031	86 - 104 - 1911316 L	19:00	-	-